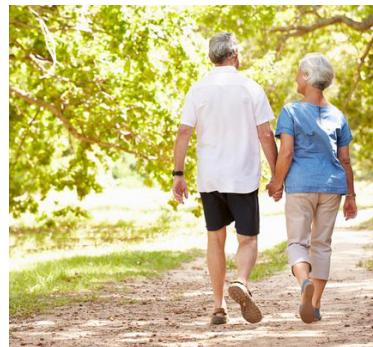


# Central Goldfields Shire Walking and Cycling Strategy Appendix



2017-2026



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**Report Date:** April 2017

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## Contents

1.0 Aim of the strategy.....	1
1.1 Scope of the strategy .....	1
2.0 Project methodology.....	2
3.0 Literature review.....	4
4.0 Demographic analysis .....	19
5.0 Our visitors .....	22
6.0 Types of walkers and cyclists in Central Goldfields Shire .....	24
7.0 Walking and cycling trends .....	27
8.0 Community consultation.....	32
8.1 Key stakeholder workshop .....	32
8.2 Key stakeholder interviews .....	37
8.3 Community workshops and listening posts .....	48
8.4 Cycling survey.....	52
8.5 Walking and running survey.....	60
8.6 Written submissions.....	68
9.0 Suggested infrastructure developments .....	70
10.0 Existing walking and cycling programs and events .....	74
11.0 Design guidelines .....	80
11.1 Stakeholder feedback on design .....	80
11.2 Provision of footpaths and cycle paths .....	81
11.3 Path width.....	83
11.4 Shared paths .....	84
11.5 Off-road walking tracks .....	85
11.6 Mountain bike tracks .....	87
12.0 Urban bike park concept.....	88
12. 1 Typical features of urban bike parks .....	90
13.0 Previous strategy achievements and required infrastructure works.....	96
14.0 Maps.....	119

## 1.0 Aim of the strategy

The aim of the Central Goldfields Shire Walking and Cycling Strategy is to provide a strategic framework to guide Council in relation to walking and cycling opportunities such as paths / trails, infrastructure, events, programs and services for the next ten years. Council is keen for the existing walking and cycling infrastructure within the municipality to be better utilised and also keen for participation rates in walking and cycling to increase. Such an increase in usage not only creates better use of existing infrastructure, but has the potential to have a profound impact on the health and wellbeing of the community.

When a network of safe, quality, linked paths and trails is created, supported by suitable programs, events and services, there is a much greater potential for local residents to utilise active transport to key destinations such as schools, work, sport and recreation facilities and shopping precincts or to enjoy recreational walking and cycling for fitness. Ultimately cycling and walking should be the easy choice for residents when considering transport options and physical activity participation. The aim of this strategy is to ensure that Central Goldfields Shire is best positioned to create a walking and cycling friendly community, within its resource capacities.

### 1.1 Scope of the strategy

Key requirements of this strategy were to:

- Develop a vision and planning principles to guide decision making in relation to the encouragement of walking and cycling.
- Assess existing plans and policies related to walking and cycling opportunities in Central Goldfields and the potential impact of these plans and policies on future trail developments.
- Audit existing walking and cycling paths / trails within the municipality.
- Provide an overview of trends and best practice in relation to walking and cycling opportunities.
- Conduct an extensive community consultation program featuring a variety of participatory opportunities to seek information from all members of the community, including traditionally disadvantaged groups, about existing and potential demands and expectations around walking and cycling.
- Provide an overview of barriers and enablers to walking and cycling in Central Goldfields Shire
- Provide an overview of gaps, constraints and opportunities which impact on the realisation of a quality integrated walking and cycling network and impact on participation levels in general
- Identify existing regulations and development of design guidelines to apply to future path / trail developments and to ensure quality path / trail experiences.
- Develop cost estimates for all walking and cycling initiatives identified.
- Identify a preferred sustainable model for governance, management, maintenance, asset renewal, development and community involvement of paths / trails.
- Develop a detailed 10 year implementation plan that includes information about prioritised strategies and actions, potential partners, timeframes and indicative costings.

## 2.0 Project methodology

The process undertaken to produce the strategy is as follows:

<i>Task</i>	<i>Details</i>
Communication Bulletin #1	Develop a Communication Bulletin to inform key stakeholders and the broader community of the project and its proposed methodology.
Literature Review	Review a range of relevant literature to assist with the understanding of previous Council and stakeholder policy directions.
Trail Audit	Undertake an audit and assessment of all walking and cycling trails / paths in the region.
Demographic Analysis	Undertake a demographic analysis to determine future population levels and growth. Consider this information in relation to participation levels in walking, cycling and running to identify potential usage and demand.
Trends Analysis	Identify and provide analysis on the current local, state, national and international trends that may impact on walking and cycling in Central Goldfields Shire.
Key Stakeholder Interviews	Meet with key stakeholders on an individual or small group basis to provide information about gaps and opportunities in relation walking and cycling opportunities.
Community survey	Design a short survey to be distributed throughout key organisations and community hubs in Central Goldfields to determine needs of both existing and potential users of paths / trails.
Five Community Workshops	Five community workshops / events will be held and documented as part of the project to gain feedback from community members about gaps and opportunities relating to walking and cycling in the region.
Development of Vision and Planning Principles	Identify a vision for walking and cycling and planning principles to guide future decision making.
Needs Analysis and Issues Paper	Undertake a needs analysis to determine the strengths and weaknesses of existing walking and cycling opportunities in Central Goldfields Shire and to identify possible opportunities and / or barriers to be addressed.
Identification of potential events, programs and services	Prepare a report on suitable events, programs and services, based on an assessment of trends, community feedback and key stakeholder feedback.
Identification of Works to be Undertaken	Identify a list of identified walking and cycling works and initiatives following the needs analysis, the audit of trails and the information received through key stakeholder and community consultations.
Preparation of Mapping	Produce an overview map of identified trails to provide a visual representation of proposed directions and individual maps for proposed key trail upgrades / developments.
Development of Costings	Prepare estimated costings for trail components and other initiatives.
Development of policy directions	Identify and briefly outline policies that need to be developed to ensure equitable development, management, maintenance of cycling and walking opportunities.
Development of Guidelines	Prepare a series of design guidelines to assist with future design and upgrades of trails, using industry standards, best practice ideas and considering local needs and

<i>Task</i>	<i>Details</i>
Development of Management and Maintenance Models	capacities. Identify a suitable and sustainable management model for each new trail development proposed, in accordance with existing approved land management plans.
Development of Report and Implementation Plan	<p>Prepare a Strategic Plan that incorporates all information obtained throughout the study in a logical, practical report format. It will provide an assessment of existing trails, identification of gaps in the network, opportunities for the development of improved opportunities for walking and cycling, management options, maintenance requirements, design guidelines, indicative costings, etc.</p> <p>Develop a detailed implementation plan to accompany the strategy and to provide guidance on future programs, events, services and facilities. It will include prioritised strategies, actions, potential partners, indicative costings and timelines. An evaluation program will also be prepared to assess the success of the strategy and to ensure that it is monitored appropriately.</p>



### 3.0 Literature review

The following 19 documents were reviewed in order to identify any particular strategic directions, policies or trends that may impact on the future provision of cycling and walking in Central Goldfields Shire:

1. Central Goldfields Shire *Council Plan 2013-2017*
2. Central Goldfields Shire *Planning Scheme*
3. Central Goldfields Shire *Bicycle Strategy Plan 2004-2013*
4. Central Goldfields Shire *Recreation Strategy 2008-2018*
5. Central Goldfields Shire *Municipal Public Health and Wellbeing Plan 2013-2017*
6. Central Goldfields Shire *The State of Play. Sport and Recreation in Central Goldfields 2014*
7. Central Goldfields Shire *Updated Sports Summit Strategies 2014*
8. Central Goldfields Shire *The way forward. Transforming sport and recreation in Central Goldfields 2014*
9. Central Goldfields Youth Advisory Council (2015) *Youth Survey Results*
10. Destination Central Goldfields *Strategic Tourism Plan 2010-2020*
11. Central Goldfields Shire, Hepburn Shire and City of Ballarat (2013) *Crossing Borders Tracks and Trails*
12. Loddon Mallee Trails Network (2010) *Loddon Mallee Regionally Significant Trails Strategy*
13. Bendigo Regional Tourism (2015) *Bendigo Region Destination Management Plan*
14. Tourism Victoria (2014) *Victoria’s Trails Strategy 2014-2024*
15. Bicycle Network Victoria (2012) *Cycling into the Future: Victoria’s Cycling Strategy 2013-2023*
16. Tourism Victoria (2011) *Victoria’s Cycle Tourism Action Plan 2011-2015*
17. Austroads *The Australian National Cycling Strategy 2011–2016*
18. Parks and Leisure Australia (2013) *Open Space Planning and Design Guide*
19. Australian Government Austrade and Tourism Research Australia (2015) *Growing Cycling Tourism in Victoria*

A summary of each document follows:

Document	Relevance
Central Goldfields Shire <i>Council Plan 2013-2017</i>	<p>The vision that guides Central Goldfields Shire is: “To be a community with choice.”</p> <p>Key actions of relevance to walking and cycling are:</p> <ul style="list-style-type: none"> <li>• Undertake traffic management study for Maryborough</li> <li>• Implement CGS Recreation Plan</li> <li>• Provide a range of facilities and programs across the municipality</li> <li>• Develop a Central Goldfields Events Strategy</li> <li>• Maintain the Council’s major asset categories (such as footpaths) to meet defined service levels including standards in accordance with Council’s Asset Management</li> </ul>



Document	Relevance
	<p>Plan</p> <ul style="list-style-type: none"> <li>• Undertake township enhancements in accordance with urban design framework.</li> </ul>
<p>Central Goldfields Shire <i>Planning Scheme</i></p>	<p>Clause 56.06 of Council’s Planning Scheme, Standard C15 notes that the municipal walking and cycling network should be designed to:</p> <ul style="list-style-type: none"> <li>• “ Implement any relevant regional and local walking and cycling strategy, plan or policy for the area set out in this scheme.</li> <li>• Link to any existing pedestrian and cycling networks.</li> <li>• Provide safe walkable distances to activity centres, community facilities, public transport stops and public open spaces.</li> <li>• Provide an interconnected and continuous network of safe, efficient and convenient footpaths, shared paths, cycle paths and cycle lanes based primarily on the network of arterial roads, neighbourhood streets and regional public open spaces.</li> <li>• Provide direct cycling routes for regional journeys to major activity centres, community facilities, public transport and other regional activities and for regional recreational cycling.</li> <li>• Ensure safe street and road crossings including the provision of traffic controls where required.</li> <li>• Provide an appropriate level of priority for pedestrians and cyclists.</li> <li>• Have natural surveillance along streets and from abutting dwellings and be designed for personal safety and security particularly at night.</li> <li>• Be accessible to people with disabilities.”</li> </ul> <p>Standard C18 of Clause 56.06 states that footpaths, shared paths, cycle paths and cycle lanes should be designed to:</p> <ul style="list-style-type: none"> <li>• “Be part of a comprehensive design of the road or street reservation.</li> <li>• Be continuous and connect.</li> <li>• Provide for public transport stops, street crossings for pedestrians and cyclists and kerb crossovers for access to lots.</li> <li>• Accommodate projected user volumes and mix.</li> <li>• Meet the requirements of Table C1 (Design of roads and neighbourhood streets –see below).</li> <li>• Provide pavement edge, kerb, channel and crossover details that support safe travel for pedestrians, footpath bound vehicles and cyclists, perform required drainage functions and are structurally sound.</li> <li>• Provide appropriate signage.</li> <li>• Be constructed to allow access to lots without damage to</li> </ul>

Document	Relevance
	<p>the footpath or shared path surfaces.</p> <ul style="list-style-type: none"> <li>• Be constructed with a durable, non-skid surface.</li> <li>• Be of a quality and durability to ensure: <ul style="list-style-type: none"> <li>○ Safe passage for pedestrians, cyclists, footpath bound vehicles and vehicles.</li> <li>○ Discharge of urban run-off.</li> <li>○ Preservation of all-weather access.</li> <li>○ Maintenance of a reasonable, comfortable riding quality.</li> <li>○ A minimum 20 year life span.</li> </ul> </li> <li>• Be accessible to people with disabilities and include tactile ground surface indicators, audible signals and kerb ramps required for the movement of people with disabilities.”</li> </ul>
<p>Central Goldfields Shire <i>Bicycle Strategy Plan 2004-2013</i></p>	<p>This strategy identified a list of prioritised capital works to improve the existing cycling network, but also focussed on measures that will allow the health and wellbeing of the community to improve, that will help to reduce vehicle emissions and promote cycle tourism in the Shire. Key focus areas include:</p> <ul style="list-style-type: none"> <li>• Capital works to 29 paths (construction or extension)</li> <li>• Signage</li> <li>• Bicycle education for cyclists and motorists</li> <li>• Cycling / walking programs, e.g. Ride to Work Day</li> <li>• Bicycle infrastructure, e.g. bike racks, lockers, seats, etc</li> <li>• Promotion and marketing</li> <li>• Commercial opportunities</li> <li>• Access</li> <li>• Interpretation</li> <li>• Management</li> <li>• Maintenance</li> <li>• Linkages</li> <li>• Review / evaluation of strategy</li> </ul> <p>An assessment of the achievements of this strategy is included in section 5.1.</p>
<p>Central Goldfields Shire <i>Recreation Strategy 2008-2018</i></p>	<p>This strategy notes:</p> <ul style="list-style-type: none"> <li>• Walking / cycling paths should be provided in Bealiba, Carisbrook, Dunolly, Maryborough and Talbot.</li> <li>• The velodrome is underutilised, but there is demand to improve facilities</li> <li>• In addition to the RACV Energy Breakthrough, CGS has attracted state, national and international competitions and training camps focussing on cycling, triathlons and mountain bike orienteering</li> <li>• The Shire could be branded as a Cycle Friendly Community</li> <li>• The quiet roads could enable more cycling events to be</li> </ul>

Document	Relevance
	<p>held locally</p> <ul style="list-style-type: none"> <li>• Cycling and walking are two of the most popular activities undertaken by Central Goldfields Shire residents</li> <li>• Cycling and walking tracks (including footpaths, roads, unmade bush tracks) are the most frequently used physical activity facility in the Shire</li> <li>• Very strong community support for the development of a network of safe bicycle / walking paths in Maryborough and between small towns</li> <li>• Some concern re lack of signage on tracks and lack of information on tracks</li> <li>• Support for extension to programs that encourage walking and cycling</li> <li>• The Golden Trail (trail recreating the walk of Chinese gold miners from Robe in South Australia to Bendigo) goes through Maryborough and Carisbrook and has potential to bring a number of touring walkers and cyclists to the area</li> <li>• Review and continue to implement recommendations from Cycling Strategy</li> <li>• Need for signs on walking paths at regular intervals and need for a cycling / walking map for the Shire</li> <li>• Improving linkage from Maryborough Swimming Pool into town via cycling / walking paths.</li> </ul> <p>The overall strategy in relation to cycling and walking is to develop a network of safe cycling and walking tracks throughout the Shire. Specific actions are:</p> <ul style="list-style-type: none"> <li>• Continue to implement the recommendations of Council's Bicycle Strategy (including development of new paths, signage and maps)</li> <li>• As new opportunities for cycling and walking are identified (e.g. an off-road shared cycling / walking path between Maryborough East and MEC), incorporate these into the existing Bicycle Strategy</li> <li>• Continue to develop the walking strategy and other active transport opportunities.</li> <li>• Work in partnership with relevant events management companies, state sporting bodies and sporting institutes to identify key infrastructure required in order to attract events and training camps to the area – particularly cycling, triathlons, mountain biking, orienteering and human powered vehicle events.</li> </ul>
<p>Central Goldfields Shire <i>Municipal Public Health and Wellbeing Plan 2013-2017</i></p>	<p>This plan notes:</p> <ul style="list-style-type: none"> <li>• 40% of adults are overweight (compared with 33% for Victoria)</li> <li>• 25% of adults are obese(compared with 17% for Victoria)</li> </ul>

Document	Relevance
	<ul style="list-style-type: none"> <li>• 64% of adults undertake sufficient physical activity to gain a health benefit [note that the amount of physical activity recommended to be undertaken by adults for health benefits has basically doubled since the PHWP was completed] (exactly the same as Victoria)</li> <li>• 81% of people feel safe walking alone at night (compared with 70% for Victoria)</li> <li>• 5.6% of people walk to work</li> <li>• 1.1% of people cycle to work</li> </ul> <p>Priorities for Council:  Priority Area Two – Increasing participation in physical activity  Goal: To improve the physical health of our community by supporting people to engage in physical activity where they live, learn, work and play:</p> <ul style="list-style-type: none"> <li>• Ensure environments for physical activity are safe, inclusive and accessible.</li> <li>• Create opportunities for people to participate in active travel, sport and active recreation</li> <li>• Develop partnerships to reduce duplication and strengthen sport and active recreation opportunities</li> <li>• Strengthen community connections through sport and active recreation</li> <li>• Support the provision of quality sport and active recreation opportunities</li> <li>• Support physical activity initiatives through Healthy Together</li> <li>• Advocate, plan and develop transport, road and pedestrian networks and connectivity between, and within, communities</li> <li>• Manage the shared use of public space for active recreation</li> <li>• Implement the Central Goldfields Shire Recreation Plan</li> <li>• Provide a range of facilities and programs across the municipality</li> <li>• Implement Recreation Reserve Master Plans</li> <li>• Maintain Council assets</li> <li>• Encourage healthy by design principles into existing and future growth area.</li> </ul>
<p>Central Goldfields Shire (2014) <i>The State of Play. Sport and Recreation in Central Goldfields</i></p>	<p>The purpose of this paper is to encourage stakeholders to think about the issues and trends facing sport and recreation clubs and organisations in the Shire, prior to a summit to be held shortly after the release of the paper. The key premise of the document is about assisting clubs and organisations to be more sustainable and increase membership.</p> <p>The paper projects that by 2031 the number of cyclists in the Shire will remain steady in the 15-24 year old age bracket, increase slightly for 25-34 year olds; decrease amongst 35-64 year olds and</p>

Document	Relevance
	<p>increase significantly by those aged over 65.</p> <p>The key roles identified for Council are fairness, facilities, consolidation, financial support and leading initiatives.</p>
<p>Central Goldfields Shire <i>Updated Sports Summit Strategies</i></p>	<p>The overall objective identified in this list of actions is: “Central Goldfields Shire community to work collaboratively and cooperatively to increase participation in active recreation and physical activity through informal and formalised opportunities.”</p> <p>Three of the most relevant actions identified in this report are:</p> <ul style="list-style-type: none"> <li>• Develop and action the 2016 - 2026 Central Goldfields Walking and Cycling Strategy to support clubs and the community with active transport linkages to sport and recreation facilities throughout the 'shire'.</li> <li>• Central Goldfields Shire to assist with the development of health and wellbeing strategies across the shire that focus on increasing participation in physical activity.</li> <li>• Community walking groups operating with support of volunteers and community and statewide organisations.</li> </ul>
<p>Central Goldfields Shire (2014) <i>The way forward. Transforming sport and recreation in Central Goldfields</i></p>	<p>The vision identified in this strategy is: “By 2035, the people of Central Goldfields will be the most active in the country and sport and active recreation will be a major driver of our towns’ liveability.”</p> <p>The three key strategies identified to achieve this vision are:</p> <ul style="list-style-type: none"> <li>• Health focussed public and private places</li> <li>• Collaboratively built active transport pathways</li> <li>• A culture of looking out for each others’ Health</li> </ul> <p>The active transport pathways component of the report states that pathways need to connect key destinations in the municipality such as schools, recreation facilities and workplaces. It is suggested that groups and organisations (such as service clubs) can build sections of the pathway and that schools and community groups can fundraise. Council’s role will be to develop, fund and manage.</p> <p>One strategy was to establish a Sport and Active Recreation Community Steering Committee. Council called for nominations in 2015 for this committee.</p>
<p>Central Goldfields Youth Advisory Council (2015) <i>Youth Survey Results</i></p>	<p>A survey was distributed to young people in 2014-15. 431 responses were collected. The following information from the survey relates directly to cycling and walking:</p> <ul style="list-style-type: none"> <li>• Three people (0.5%) said that they had been members of a bike club in the past and no one stated that they are</li> </ul>

Document	Relevance
	<p>currently a member of a bike club.</p> <ul style="list-style-type: none"> <li>• Twenty-three people (2.6%) said that they currently take part in BMX, mountain bike riding or cycling and nine people (1%) said that they run.</li> <li>• In terms of activities people would like to do: 5 people said that they would like to be part of a mountain bike club and ride on mountain bike trails; 4 people said that they would like bike tracks or on road lanes for bicycles; 1 would like a better BMX track; another would like better bike jumps; 3 people said that they would like to take part in bike riding; and 1 person said that they would like to walk.</li> <li>• 26 people like the Energy Breakthrough event.</li> <li>• There were several comments about people not feeling safe on the streets at night</li> <li>• In order to communicate with young people, facebook is the most popular medium, followed by email.</li> </ul>
<p>Destination Central Goldfields <i>Strategic Tourism Plan 2010-2020</i></p>	<p>Key points of relevance from this document:</p> <ul style="list-style-type: none"> <li>• The majority of visitors to the Shire come from people living in Melbourne who come to visit friends and relatives for 1-2 nights. They occasionally eat out and their yield is low.</li> <li>• The Shire could potentially attract the 'Traditional Family Visitor', 'Socially Aware' and 'Visible Achiever' market segments</li> <li>• The Highland Gathering and the RACV Energy Breakthrough are the only two events that attract a reasonable level of tourism revenue.</li> <li>• Potential to market Central Goldfields Shire as a 'short break' or 'overnight stop' for people travelling between Melbourne and South Australia.</li> </ul> <p>The report notes:</p> <ul style="list-style-type: none"> <li>• CGS is well positioned to become a centre of cycling due to its mostly flat roads that are easy to access and the short distances between towns</li> <li>• Cycling tours may be of interest to Families, Young Optimists and Eco tourist segments.</li> <li>• A bicycle hire facility could be established at Maryborough Railway Station</li> <li>• A bicycle trails brochure should be produced for visitors and it should be available at the bicycle hire facility</li> <li>• A Walking the Arts Precinct Brochure should be developed with dissemination points at Maryborough Railway Station, the Visitor Information Centre and Fire Station Art Gallery.</li> <li>• A Talking History Trail with audio CD and accompanying Trail map could be made available for purchase or hire.</li> <li>• There should be a series of historic, recreational and family oriented activity trails developed and accompanied by brochures. The emphasis will be on the three major towns</li> </ul>

Document	Relevance
	<p>(Dunolly, Maryborough and Talbot) and the links between them.</p> <ul style="list-style-type: none"> <li>• Signage will be important when developing trails.</li> </ul> <p>A specific strategy recommended in the plan is: “Capitalise on Railway Domain Development / Arts Precinct Development / cycling / walking tours and Shire events to promote Central Goldfield Shire ‘Liveability’ theme.”</p>
<p>Central Goldfields Shire, Hepburn Shire and City of Ballarat (2013) <i>Crossing Borders Tracks and Trails</i></p>	<p>The trail of most relevance identified in this strategy is the proposed Ballarat-Maryborough Heritage Trail. This is essentially on-road cycling path which connects Maryborough to Ballarat via Creswick and several other small towns, utilising back-country roads. It is a joint initiative of Central Goldfields Shire, Hepburn Shire and City of Ballarat.</p>
<p>Loddon Mallee Trails Network (2010) <i>Loddon Mallee Regionally Significant Trails Strategy</i></p>	<p>This strategy identifies the potential to develop an on-road cycle touring trail from Maryborough to Ballarat, known as the Cobb and Co Cycle Touring Trail. The proposed trail is 86km in length and would use a mix of dirt, gravel and asphalt roads. Towns along the route would include Maryborough, Talbot, Clunes and Ballarat, with an onward connection to the Skipton Rail Trail or the Goldfields Track. This trail was estimated to cost approximately \$350,000 for signage and estimated to attract 4,000 users annually with an economic contribution of approximately \$120-\$150K annually. The report notes that the return of passenger rail between Maryborough and Ballarat will allow trail users to potentially travel one section by rail and the other by road. If passenger rail is also able to be returned to Talbot and Clunes, the potential use of this trail, using a combination of foot / bicycle and train, will further increase. It also notes that it may be possible to develop sections of this trail as an off-road experience in the future if funds are available and if potential demand exists.</p>
<p>Bendigo Regional Tourism (2015) <i>Bendigo Region Destination Management Plan</i></p>	<p>Cycling and nature based tourism and adventure are identified as a secondary product for Central Goldfields, meaning that whilst it's considered a strength of the Shire it is not considered to be the number one tourism product. However, it is noted that there is scope to further develop cycling and nature based tourism and adventure to improve the product, awareness and profile. Central Goldfield's primary strengths are identified as festivals and events; retail; and history and heritage. For Central Goldfields specifically, improvement of infrastructure and nature based tourism offerings in State and Regional parks is a destination development priority.</p> <p>The report notes the following priorities in relation to walking and cycling in the Shire:</p> <ul style="list-style-type: none"> <li>• Creation of a cultural arts precinct which links the key heritage sites in Maryborough including the Town Hall, Art Gallery, and Train Station (these areas could be linked by a</li> </ul>



Document	Relevance
	<p>walking route)</p> <ul style="list-style-type: none"> <li>• The Central Goldfields is well positioned for road cycling with its flat topography and short distances between historic towns. The development and promotion of road cycling routes linking historic towns.</li> <li>• Consider new uses for the disused rail line linking Maryborough and Castlemaine (e.g. rail trail)</li> <li>• Work with Parks Victoria to improve visitor facilities and interpretation within State and Regional Parks – including Paddy’s Ranges Walking Track</li> <li>• Central Goldfields Village Walking Trails - Develop a mini-series of walking trails throughout each of the Central Goldfields Villages through the natural and built landscape telling the stories of each village.</li> <li>• Carisbrook Creek Foreshore Improvements - landscaping works to improve the foreshore along with the development of a signed walking trail.</li> <li>• Moliagul Historic Reserve Facilities Upgrade- review the Moliagul Historic Reserve facilities and support the upgrade of signage on the Welcome Stranger Discovery Walk in conjunction with Parks Victoria and other relevant bodies.</li> <li>• Ballarat – Maryborough Heritage Trail – further develop this trail to provide an on-road cycling connection utilising back-country roads between Maryborough to Ballarat (via Creswick and other small towns)</li> </ul> <p>‘Ride Goldfields’ is a regional product that has been listed as a priority project in this report. Although it doesn’t specifically identify any new trail developments for CGS, it mentions the need to develop a website and marketing campaign to showcase cycling experiences, acquire new events, encourage the establishment of cycling related businesses to support visitors and to accredit bicycle friendly accommodation.</p> <p>Almost one third of ‘lifestyle leaders’ surveyed re the Bendigo region are very interested in experiencing nature through recreational and outdoor activities such as bushwalking (32%), with another 39% somewhat interested. A much smaller percentage is very interested in cycling - including recreation road and mountain biking (13%), with another 18% somewhat interested.</p> <p>In total there are 624 bed spaces spread across 37 establishments in Central Goldfields Shire. In terms of accommodation options, caravan parks are the key facility:</p> <ul style="list-style-type: none"> <li>• Caravan parks – 4 facilities, 228 beds (cabins only – does not include campsites)and 37% of all bed spaces</li> <li>• Motels – 7 facilities, 181 beds, 29% of all bed spaces</li> <li>• Self contained accommodation – 14 facilities, 124 beds, 20% of all bed spaces</li> </ul>

Document	Relevance
	<ul style="list-style-type: none"> <li>• B&amp;B's – 8 facilities, 49 beds, 8% of all bed spaces</li> <li>• Farm stay – 2 facilities, 22 beds, 4% of all bed spaces</li> <li>• Hotel – 2 facilities, 20 beds, 3% of all bed spaces.</li> </ul> <p>Bed spaces by town are as follows:</p> <ul style="list-style-type: none"> <li>• Maryborough – 303 or 49%</li> <li>• Laanecoorie – 80 or 13%</li> <li>• Dunolly – 79 or 13%</li> <li>• Other – 55 or 9%</li> <li>• Talbot – 42 or 7%</li> <li>• Carisbrook – 33 or 5%</li> <li>• Daisy Hill – 18 or 3%</li> <li>• Amherst – 14 or 2%</li> </ul>
<p>Tourism Victoria (2014) <i>Victoria's Trails Strategy 2014-2024</i></p>	<p>The vision outlined in this document is “ for Victoria to be recognised as a leading trail-based destination that provides a diverse range of quality trail experiences for visitors, while strengthening the State's economy and improving the health, wellbeing and lifestyle of the community.”</p> <p>In order to achieve this vision, the key initiatives to be undertaken include:</p> <ul style="list-style-type: none"> <li>• improve the quality of trail experiences in Victoria</li> <li>• increase awareness and visitation to Victorian trails</li> <li>• support complementary tourism and retail businesses</li> <li>• seek improved understanding of trail-user markets, their motivations and influences.</li> </ul> <p>The five key directions outlined in this statewide document are:</p> <ol style="list-style-type: none"> <li>1. Provide a strategic framework for trail investments</li> <li>2. Support effective planning, development and maintenance of trails</li> <li>3. Provide high quality information on trails</li> <li>4. Create better trail experiences</li> <li>5. Market trails.</li> </ol> <p>The strategy notes:</p> <ul style="list-style-type: none"> <li>• Local Government and Committees of Management manage 1,200 km or 15% of the trail network</li> <li>• Parks Victoria manages 4,000 km or 50% of recreational trails</li> <li>• Department of Environment, Land, Water and Planning manages 2,800 km or 35% of the network.</li> </ul> <p>It also notes that:</p> <p>“To realise the potential economic and social benefits of trails,</p>

Document	Relevance
	<p>Victoria needs to create quality visitor experiences that match users' expectations. Therefore, the way we think about trail development needs to change significantly – encompassing trail infrastructure as well as related products and services, consumer information and marketing activities.”</p> <p>The report also notes:</p> <p>“The broad and growing appeal of trails to domestic and international visitor markets, as well as local residents for recreational purposes, means Victoria’s trails have great potential to boost regional economies and generate business opportunities and jobs if they are developed into quality, well-marketed tourism experiences.”</p> <p>Four key areas identified in the strategy to gain maximum benefits from trail networks through increased usage and visitation are:</p> <ul style="list-style-type: none"> <li>• investment</li> <li>• development of infrastructure and complementary tourism products</li> <li>• consumer information provision</li> <li>• marketing.</li> </ul> <p>The strategy notes:</p> <p>“Trail-based events, such as mountain bike competitions and adventure races, are gaining in popularity and can attract significant visitor numbers to regional locations. Competitors and their families, spectators and organisers can boost the demand for accommodation, food and beverage, and other services.”</p> <p>Another key point to note is that in 2012 83% of leisure travellers in the US used the internet to research and plan their travel. Hence an investment in an online presence is imperative.</p> <p>The State Government will use the following criteria (pg 16) to prioritise trail investments:</p> <ul style="list-style-type: none"> <li>• Trail user profile – define target market and establish trail user profile</li> <li>• Quality – experience that meets the needs and expectations of users, adherence to standards, supporting infrastructure and services.</li> <li>• Access – for people of all abilities as appropriate and to towns, public transport, etc</li> <li>• Safety – risks identified and managed</li> <li>• Economic benefits - demonstrated</li> <li>• Environment and heritage benefits - demonstrated</li> <li>• Social benefits - demonstrated</li> </ul>

Document	Relevance
	<ul style="list-style-type: none"> <li>• Demand - identified</li> <li>• Support by landowners and / or land managers</li> <li>• Development costs – design and construction costs identified</li> <li>• Long term commitment of resources – to manage, maintain and promote</li> <li>• Natural and cultural values – minimal impact and managed as appropriate</li> <li>• Planning requirements – identified and addressed</li> <li>• Strategic imperative – supported by plans.</li> </ul>
<p>Bicycle Network Victoria (2012) <i>Cycling into the Future: Victoria's Cycling Strategy 2013-2023</i></p>	<p>The six key directions identified in this strategy are:</p> <ol style="list-style-type: none"> <li>1. Build evidence</li> <li>2. Enhance governance and streamline processes</li> <li>3. Reduce safety risks</li> <li>4. Encourage cycling</li> <li>5. Grow the cycling economy</li> <li>6. Plan networks and prioritise investments.</li> </ol> <p>This strategy notes:</p> <ul style="list-style-type: none"> <li>• More than 1.08 million (19%) Victorians ride a bike each week</li> <li>• Up to 42% of Victorians ride a bike once per year</li> <li>• Almost 2 out of 3 households in Victoria own a bike</li> <li>• More bikes are sold in Australia each year than cars</li> <li>• Only 1.6% of transport trips are on bike</li> <li>• Only 3% of trips to work and school are undertaken by bike</li> <li>• Around 10,000 people are employed in the Australian cycling industry.</li> <li>• There is a greater opportunity for cycling to be used more frequently for shorter trips to replace either a public transport or motor vehicle trip</li> <li>• Nearly three-quarters of trips between one and two kilometres are currently made by car.</li> <li>• In 1970, about 84 per cent of children walked, cycled or used public transport to get to school. In 2005, 91 per cent of children were driven to school.</li> <li>• Nationally, cycling tourism and events are estimated to generate \$254 million each year for local economies.</li> <li>• In the five years to 2011, the average number of cycling fatalities each year was 7.4 (two per cent of all road deaths) and an average of 454 bike riders were seriously injured each year (seven per cent of all serious injuries on the roads reported to police)</li> <li>• The SmartRoads approach to be adopted by the State Government will support cycling by allocating road space and providing traffic signal priority to bike riders where</li> </ul>

Document	Relevance
	<p>appropriate.</p> <ul style="list-style-type: none"> <li>• An Australian study found that 25 per cent of respondents felt that cycling was not a safe form of transport and 67 per cent of respondents cited unsafe road conditions as a reason for not cycling more frequently.</li> <li>• Successful cycle tourism is dependent on the total tourism experience rather than just the cycling element. This includes where the trail is located, how easy it is to access, associated tourism products (such as places to see and things to do), whether there are good directions, signs and maps and secure places to store or park bikes. Bike-friendly places to stay overnight are also important.</li> <li>• Cycling will play an increasingly important role in meeting transport needs and supporting vibrant, healthy urban communities in regional Victoria. This will be reflected in the development of Regional Growth Plans.</li> <li>• The Victorian Government will focus on supporting the construction of high quality regional trails and specialist sporting facilities that are significant to the state.</li> <li>• The Victorian Planning Provisions integrate planning for cycling with land use and development planning, including the provision of end-of-trip facilities such as bike parking and showers in major multi-story developments.</li> </ul>
<p>Tourism Victoria (2011) <i>Victoria's Cycle Tourism Action Plan 2011-2015</i></p>	<p>Although now out of date, this report focussed on three key areas:</p> <ul style="list-style-type: none"> <li>• Strengthening the supply of cycle tourism experiences</li> <li>• Building consumer demand for cycle tourism experiences</li> <li>• Attracting and leveraging events</li> </ul> <p>This strategy identifies the strengths of the Goldfields region of Victoria as: trails and major cycling events. Its secondary strengths are road cycling and mountain bike riding.</p> <p>The report notes that the target market for tracks and trails is:</p> <p>“Predominantly couples aged fifty plus and families with children, who seek an experience that allows them to spend quality time with friends and family. This market is interested in complementary experiences, such as food and wine and exploring Victoria’s villages. Currently these visitors are predominantly intrastate travellers, with the interstate market offering considerable potential due to Victoria’s unique product offering, including quality boutique accommodation, food and wine and other complementary experiences.”</p> <p>It notes that the target market for cycling events is:</p> <p>“Professional cycling events bring a high number of spectators. Non-professional events interest mainly participants. This segment</p>

Document	Relevance
	<p>is mainly comprised of males aged 40-54 years, who are working professionals, well educated and have a high disposable income. This market appreciates the health and fitness benefits of cycling. Some cycling events, both professional and non-professional, currently attract a high percentage of interstate visitors, especially from NSW and the ACT.”</p>
<p>Austrroads <i>The Australian National Cycling Strategy 2011–2016</i></p>	<p>The vision of Austrroads Australia, as articulated in this strategy, is to double the number of people cycling from 2011 to 2016 so that individuals and communities can enjoy the benefits of cycling. There are six key priorities identified in the strategy:</p> <ol style="list-style-type: none"> <li>1. Promoting cycling as a viable and safe mode of transport, and an enjoyable recreational activity.</li> <li>2. Creating a comprehensive and continuous network of safe and attractive routes to cycle and end-of-trip facilities.</li> <li>3. Addressing cycling needs in all relevant transport and land use planning activities.</li> <li>4. Enabling people to cycle safely.</li> <li>5. Improving monitoring and evaluation of cycling programs.</li> <li>6. Developing nationally consistent guidance for stakeholders.</li> </ol>
<p>Parks and Leisure Australia (2013) <i>Open Space Planning and Design Guide</i></p>	<p>This strategy details the types of open space in which walking and cycling typically occur, e.g. linear open space and trails.</p> <p>The document notes that an application for a sub-division that creates streets or open space (under Clause 56.05-2 Public Open Space Provision Objectives) should be accompanied by a landscape design that also features the provision of walking and cycling networks that link with community facilities. Also under this clause, and standard C13, it states that local parks should be located within 400m safe walking distance of at least 95 percent or all dwellings.</p> <p>The document notes several clauses with the Victorian Planning Provisions that relate to walking and cycling, namely:</p> <ul style="list-style-type: none"> <li>• Clause 56.06-2 Walking and cycling network</li> <li>• Clause 56.06-5 Walking and cycling network detail objectives.</li> </ul>
<p>Australian Government Austrade and Tourism Research Australia (2015) <i>Growing Cycling Tourism in Victoria</i></p>	<p>The key messages from this research are:</p> <ul style="list-style-type: none"> <li>• The cycle tourism market is relatively small, but those involved often take multiple trips per year and are highly engaged.</li> <li>• Cycle tourists mostly visit Melbourne and Sydney, but often travel to regional areas</li> <li>• Cycle tourists engage in a broad range of activities</li> <li>• Victoria rates highly in the following areas for cycle tourists:</li> </ul>

Document	Relevance
	<p>facilities, accommodation, variety of cycling options and quality of tracks and trails</p> <ul style="list-style-type: none"> <li>• The knowledge of cycle tourism by those who do not participate is low</li> <li>• Online information can generate awareness and educate the market on Victoria as a cycling destination.</li> </ul> <p>Of those people interviewed as part of this research who take part in cycle tourism:</p> <ul style="list-style-type: none"> <li>• 41% undertake day trips</li> <li>• 40% spend 1-3 nights on their trip</li> <li>• 18% spend 4+ nights on their trip</li> <li>• 45% of cycle tourists undertake a cycling holiday at least 3 times per year</li> <li>• 41% enjoy sightseeing on the trip</li> <li>• 35% cycle on the foreshore or a river or lake</li> <li>• 38% want downloadable maps</li> <li>• 37% want accessible online information</li> <li>• 37% want extensive cycle paths and trails</li> <li>• 36% want websites that provide comprehensive information for planning a trip.</li> </ul> <p>The report notes the need to:</p> <ul style="list-style-type: none"> <li>• Give a reason to go</li> <li>• Give an online experience that provides simple information</li> <li>• Ensure safety and conveniences</li> <li>• Augment experiences that highlight natural surroundings</li> <li>• Promote the dedicated paths and facilities</li> <li>• Persuade potential travellers of the benefits</li> <li>• Remind potential participants of the beauty of Victoria's regional areas</li> <li>• Engage image-based communications via regional tourism channels</li> <li>• Encourage packaged tours</li> <li>• Create tours that align with specific events, times or destinations</li> <li>• Build and promote dedicated cycling tourism facilities.</li> </ul>



## 4.0 Demographic analysis

Demographic data provides information which can be used by organisations to understand the current make-up of the community. Potential markets can be determined by examining the number of people in specific age groups. Other details such as household income may help decision making in terms of setting fees and languages spoken at home may indicate a need to have information translated into other languages for example.

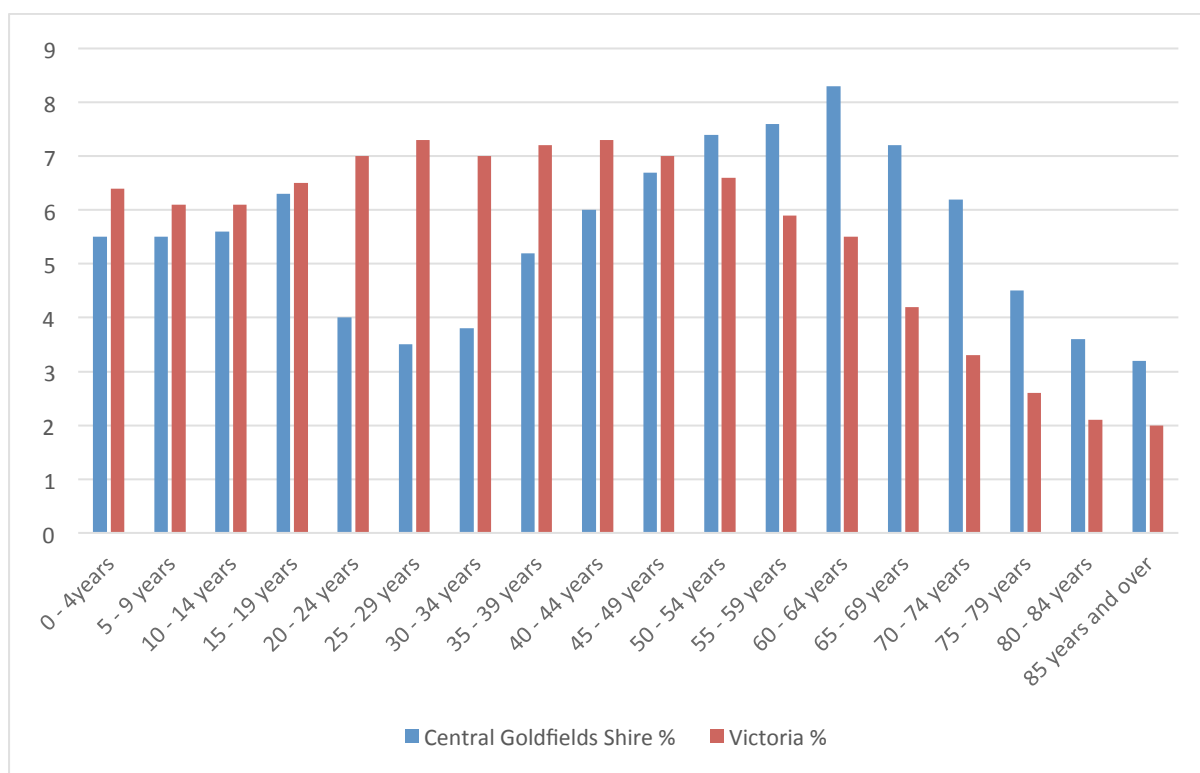
A brief summary of the demographic make-up of Central Goldfields Shire follows<sup>1</sup>.

### *Population and age*

Central Goldfields comprises a total population of 12,496 people – 50% males, 50% female. The median age is 48, compared with 37 for both Victoria and Australia.

There are 3,384 families in Central Goldfields Shire (47% without children, 34% with children, 18% one parent families and 1% other types of families). The average number of children per family is 1.9.

Central Goldfields Shire has a much higher percentage of people over the age of 50 years of age (48% over the age of 50 years compared with 32% for Victoria) and a significantly lower percentage of people aged 20 – 34 years (11% compared with 21% for Victoria). The percentage of young people from 0-19 years of age is relatively similar to the Victorian figures, as is the percentage of people aged from 40-49 years of age. In general, the figures for rural Victoria are very similar to those for Central Goldfields Shire.



1

[http://www.censusdata.abs.gov.au/census\\_services/getproduct/census/2011/quickstat/LGA21670?opendocument&navpos=220](http://www.censusdata.abs.gov.au/census_services/getproduct/census/2011/quickstat/LGA21670?opendocument&navpos=220) (viewed 25<sup>th</sup> January 2016)

The population and median age for each Australian Bureau of Statistics 'state suburb' within Central Goldfields Shire is as follows:

Township	State Suburb population	% Female	% Male	Median age
Bealiba <sup>2</sup>	300	47.3	52.7	51
Carisbrook <sup>3</sup>	1,143	49.1	50.9	44
Dunolly <sup>4</sup>	908	47.2	52.8	55
Maryborough <sup>5</sup>	7,630	51.7	48.3	48
Talbot <sup>6</sup>	715	49.4	50.6	48

Between 2011 – 2031, there is expected to be a net increase of 286 local residents, based on an average annual growth rate of 0.4%<sup>7</sup>. Central Goldfields Shire is expected to comprise of approximately 13,768 residents by 2031. However, this figure could change if circumstances change, e.g. a new major employer relocates to Maryborough or there is an influx of recently arrived people / refugees to the area. By 2031, 56% of the population is expected to be over 50 years of age.

### *Birthplace and languages spoken*

The majority of residents were born in Australia (87% compared with a national figure of 70% and a state figure of 69%). 4% of local residents were born in England, 1% in New Zealand and 1% born in Netherlands. People born in Scotland make up 0.4% of the population and those born in Germany make up 0.3% of the population.

The majority of residents only speak English at home (95%). Of those who speak languages other than English at home, the most common languages are Dutch, Italian and German (all 0.2%).

2

[http://www.censusdata.abs.gov.au/census\\_services/getproduct/census/2011/quickstat/SSC20096?opendocument&navpos=220](http://www.censusdata.abs.gov.au/census_services/getproduct/census/2011/quickstat/SSC20096?opendocument&navpos=220)  
(viewed 25<sup>th</sup> January 2016)

3

[http://www.censusdata.abs.gov.au/census\\_services/getproduct/census/2011/quickstat/SSC20257?opendocument&navpos=220](http://www.censusdata.abs.gov.au/census_services/getproduct/census/2011/quickstat/SSC20257?opendocument&navpos=220) (viewed 25<sup>th</sup> January 2016)

4

[http://www.censusdata.abs.gov.au/census\\_services/getproduct/census/2011/quickstat/SSC20426?opendocument&navpos=220](http://www.censusdata.abs.gov.au/census_services/getproduct/census/2011/quickstat/SSC20426?opendocument&navpos=220) (viewed 25<sup>th</sup> January 2016)

5

[http://www.censusdata.abs.gov.au/census\\_services/getproduct/census/2011/quickstat/SSC20853?opendocument&navpos=220](http://www.censusdata.abs.gov.au/census_services/getproduct/census/2011/quickstat/SSC20853?opendocument&navpos=220) (viewed 25<sup>th</sup> January 2016)

6

[http://www.censusdata.abs.gov.au/census\\_services/getproduct/census/2011/quickstat/SSC21294?opendocument&navpos=220](http://www.censusdata.abs.gov.au/census_services/getproduct/census/2011/quickstat/SSC21294?opendocument&navpos=220) (viewed 25<sup>th</sup> January 2016)

<sup>7</sup> [http://www.delwp.vic.gov.au/\\_data/assets/excel\\_doc/0008/308339/Central-Goldfields-S\\_VIF2015\\_One\\_Page\\_Profile.xlsx](http://www.delwp.vic.gov.au/_data/assets/excel_doc/0008/308339/Central-Goldfields-S_VIF2015_One_Page_Profile.xlsx) (viewed 25th January 2016)

## *Employment and income*

23% of people over 15 years of age in Central Goldfields carried out voluntary work for an organisation in the last 12 months, compared with 18% for Victoria.

4,744 people were in the labour force at the time of the 2011 ABS Census. 52% of these people worked full-time; 34% worked part-time; 8% were away from work; and 7% were unemployed. Compared with Victorian statistics, there are slightly less people working full-time (52% compared to 59%) in Central Goldfields, slightly more working part-time (34% compared to 30% in Victoria) and more people unemployed (7% Central Goldfields compared to 5% Victoria).

Key employment industry sectors are hospitals (5%); printing and printing support services (5%); school education (5%); supermarket and grocery stores (5%) and sheep, beef cattle and grain farming (5%).

The highest category of occupational types in 2011 is Technicians and Trade Workers (17% compared to 14% in Victoria overall, followed by labourers (16%), managers (13%), professionals (12%), sales workers (11%), community and personal service workers (11%), clerical and administrative workers (10%) and machinery operators and drivers (8%).

The median weekly household income is \$685 compared with the Victorian median household income of \$1,216. 43% of Central Goldfields earn less than \$600 gross per week (compared with 24% of Victorian households) and 2% earn more than \$3,000 gross per week (compared with 10% of Victorian households).

## *Households*

63.7% of households in Central Goldfields are family households (compared with 71% for Victoria). The percentage of lone households in Central Goldfields is higher than for Victoria (34% compared with 25% for Victoria).

The most common form of family household composition is a couple family without children (47%) compared with Victoria (37%), followed by a couple family with children (34%) compared to (46%) Victorian figures.

## *Home ownership*

There is a greater percentage of residents who own their homes in Central Goldfields (74% - i.e. owned outright, or owned with a mortgage compared to 70% for Victoria. There is a lower percentage of homes rented (21% compared with 27% for Victoria).

For those paying off homes, the median loan repayment in Central Goldfields is \$867 per month compared with \$1,700 for Victoria overall and for those who are renting homes, the median weekly rental amount is \$150 compared with the Victorian median of \$277.

## *Transportation*

There is an average of 1.8 motor vehicles per dwelling.

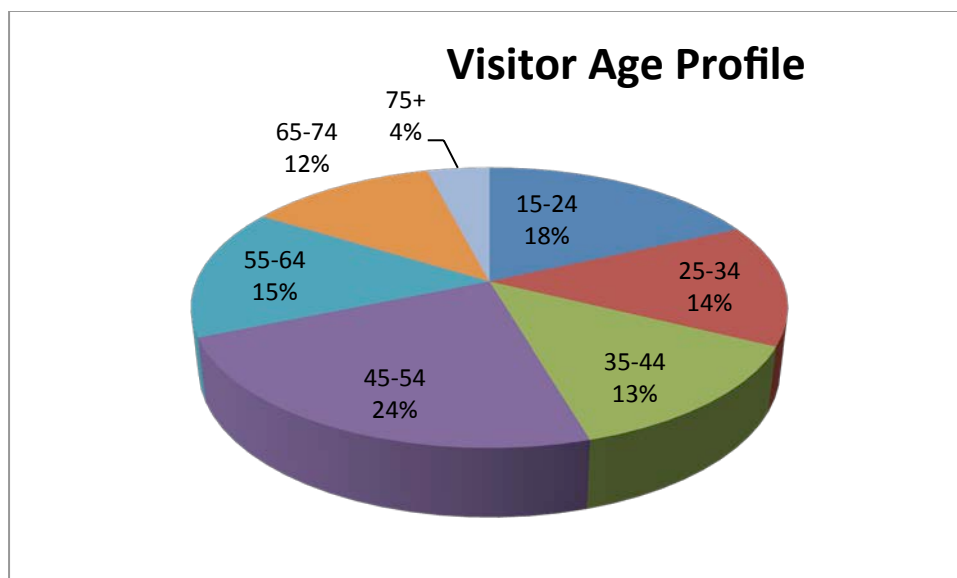
## 5.0 Our visitors

Central Goldfields Shire receives approximately 53,900 overnight visitors annually and 139,830 day visitors<sup>8</sup>. These visitors may take part in walking and cycling while here, or may be attracted to the area specifically to take part in such activities, thereby bringing additional revenue to local businesses.

For those spending one or more nights in the Shire, visiting friends and relatives is the most common reason (53%), followed by having a holiday (35%). Only 6% of overnight visitors stay in Central Goldfields Shire for business purposes, and another 6% stay for other reasons. Accommodation for overnight visitors in Central Goldfields Shire is predominantly at the property of friends or relatives (55%), followed by caravan / camping / motor home / cabin park (20%), standard hotel or motor inn rated less than 4 stars (15%), a rented house / apartment, flat or unit that is not serviced daily (4%), a guest house / bed and breakfast (4%), a luxury hotel / luxury resort rated at 4 or 5 stars (1%), their own holiday home (1%).

5% of domestic overnight visitors to the Central Goldfields area are likely to go cycling during their visit. This compares with a regional Victorian average of 3% and figures of only 1% for Bendigo. 14% of domestic overnight visitors are also likely to go bushwalking compared with the regional Victorian average of 14% and the Bendigo figure of 5%. An even higher percentage of domestic overnight visitors to Central Goldfields Shire are likely to cycle or bushwalk than other neighbouring municipalities. Hence there may be opportunities to develop opportunities that cross municipal boundaries.

The largest age cohort of visitors to Central Goldfields is the 45-54 year age group followed by 15-24 year olds. There is a fairly even distribution of all other age cohorts, with those aged over 75 least represented.



<sup>8</sup> Urban Enterprise (2015) Mair Street Spine Precinct Master Plan Draft

In terms of day trippers, the majority of visitors to Central Goldfields come from Bendigo (34%), followed by other areas of regional Victoria (27%). Melbourne accounts for 25% of visitors to the area and Ballarat 13%. The final 1% of visitors comes from interstate (most likely South Australia).

Day trippers are most likely to come to Central Goldfields Shire to visit friends and relatives (45%), or to come for a one day holiday (39%). 8% come to the area for business purposes and another 8% for other reasons.

In terms of international visitors, there is no data available for Central Goldfields Shire. However, the origin of international visitors to the Bendigo region (of which Central Goldfields is a part) are predominantly New Zealand (22%), UK (21%) and United States of America (9%). The majority of these visitors come to the Bendigo region for a holiday (42%) or to see friends and family (35%).

Between 2004 and 2014, domestic day visitation rate in the Bendigo region grew by 4%, domestic overnight visitation grew by 1%, international visitation by 7%. This equates to a visitation growth rate of 3% in the region.

Given that the majority of visitors to the Shire come to see family and friends, it is important that any marketing campaigns involve educating local people about suitable locations to walk and cycle. There also appears to be a greater propensity for visitors to the area to go bushwalking or to ride a bike than in other areas such as Bendigo, hence ensuring that information is available to potential visitors in their trip planning period will also be important to capitalise on this interest or desire. The internet and social media will be key tools in marketing the walking and cycling opportunities in the Shire, along with printed materials to a lesser extent.

## 6.0 Types of walkers and cyclists in Central Goldfields Shire

The following table (adapted from Austroads<sup>9</sup>) describes the characteristics of different types of walkers and cyclists:

Category	Riding characteristics	Walking characteristics	Riding environment	Walking environment
<b>Primary school children</b>	Cognitive skills not developed, little knowledge of road rules, require supervision.	Cognitive skills not developed, little knowledge of road rules, require supervision.	Off-road path, footpath (where permitted) or very low volume residential street.	Footpaths or off-road shared paths.
<b>Secondary school children</b>	Skill varies, developing confidence.	Generally fairly competent walkers.	Generally use on-road facilities or off-road paths where available.	Footpaths or off-road shared paths.
<b>Recreational</b>	Experience, age, skills vary greatly.	Generally fairly competent walkers. Some may be commencing a rehabilitation program and others may walk for fitness. Some will walk in parks on their holidays. Some will be highly skilled bushwalkers.	Desire off-road paths and quiet local streets, avoid heavily trafficked routes, more experienced will prefer to use road system for long journeys. Mountain bike riders will utilise bush tracks or specifically built trails.	Footpaths, off-road shared paths and bush tracks.
<b>Commuter</b>	Vary in age, skill and fitness, some highly skilled and able to handle a variety of traffic conditions.	Generally fairly competent walkers. Some will be quite fast.	Some prefer paths or low-stress roads, willing to take longer to get to destination, others want quick trips regardless of traffic conditions, primarily require space to ride and smooth riding surface, speed maintenance.	Primarily footpaths, but may also use off-road paths if sufficiently direct.
<b>Utility</b>	Ride for specific purposes (shopping), short length trips, routes	Generally fairly competent walkers.	Not on highly trafficked roads, needs include comprehensive, low-stress routes,	Primarily footpaths, but may also use off-road paths at times.

<sup>9</sup> Austroads (2014) Cycling Aspects of Austroads Guides

Category	Riding characteristics	Walking characteristics	Riding environment	Walking environment
	unpredictable.		appropriate end-of-trip facilities	
<b>Touring</b>	Long distance journeys, may be heavily equipped, some travelling in groups.	N/A	Often route is similar to that of other tourists.	N/A
<b>Sporting</b>	Often in groups, two abreast occupying left lane, needs similar to commuters.	N/A	Travel long distances in training on arterials, may include challenging terrain in outer urban or rural areas, generally do not use off-road routes because of high speed and conflict with other users.	N/A

Survey respondents from Central Goldfields Shire tend to identify with the following types of walkers or cyclists:

Walker	Cyclist
<ul style="list-style-type: none"> <li>Recreational walker – mostly on off road tracks/paths to key destinations such as parks/playgrounds, lakes/rivers, bush, sports facilities, other attractions, etc. These destinations may be local or may be in other places, e.g. when I’m on holiday (58%)</li> </ul>	<ul style="list-style-type: none"> <li>Fitness cyclist – long distance training (49%)</li> </ul>
<ul style="list-style-type: none"> <li>Utility walker – I walk as a form of transport, e.g. to the shops, rather than for recreational purposes (29%)</li> </ul>	<ul style="list-style-type: none"> <li>Recreational cyclist – riding on off road trails (39%)</li> </ul>
<ul style="list-style-type: none"> <li>Fitness walker/runner – walk or run long distances regularly as part of my training (24%)</li> </ul>	<ul style="list-style-type: none"> <li>Utility cyclist – for transport (29%)</li> </ul>
<ul style="list-style-type: none"> <li>Other (24%) – included answers such as push pram around streets, dog walker, slow walker, at gym on the treadmill, fast walker, walk to visit friends, etc</li> </ul>	<ul style="list-style-type: none"> <li>Commuter cyclist – to work or school (24%)</li> </ul>
<ul style="list-style-type: none"> <li>Commuter walker – walk to work or school primarily on footpaths (20%)</li> </ul>	<ul style="list-style-type: none"> <li>Mountain bike rider – mountain bike trails (20%)</li> </ul>



<ul style="list-style-type: none"> <li>Recreational jogger/runner – jog/run mostly on off road tracks/paths to or around key destinations such as parks/playgrounds, lakes/ivers, bush, sports facilities, etc. (20%)</li> </ul>	<ul style="list-style-type: none"> <li>Cycle tourist – ride while on holidays (17%)</li> </ul>
	<ul style="list-style-type: none"> <li>Competitive cyclist – take part in races (15%)</li> </ul>
	<ul style="list-style-type: none"> <li>Touring cyclist – go on cycling holidays (10%)</li> </ul>

## 7.0 Walking and cycling trends

### Trend

### Details

#### *Bicycle share schemes*



Bicycle share schemes have been established in larger cities such as Melbourne. They allow people to hire bikes from docking stations distributed around the city and encourage short trips to be taken by bicycle rather than by public transport or vehicle.

Source: Wikipedia:

[https://en.wikipedia.org/wiki/Cycling\\_in\\_Melbourne#/media/File:Melbournebikeshare\\_station\\_Macarthur\\_St\\_2010.jpg](https://en.wikipedia.org/wiki/Cycling_in_Melbourne#/media/File:Melbournebikeshare_station_Macarthur_St_2010.jpg)

#### *Fleet bikes*

More and more government agencies, businesses and hotels are using a fleet of bicycles for their staff to reduce the use of vehicles over short distance or for visitors as a value added component to their stay.

#### *Electric bikes / e-bikes*



Electric bikes or e-bikes have a small electric motor and rechargeable batteries. Although the bikes can be pedaled like a regular bike, they provide additional power to the rider, and are popular in areas where the terrain varies. The motors on electric bikes cut out at 25kmph in Australia (but faster speeds can be achieved by pedaling). Sales of e-bikes internationally continue to rise. The number of e-bikes produced in China in 2014 was estimated to exceed 200 million. E-bikes in Australia can be purchased from just over \$1,000 upwards.

Source: INSG Insight (2014) the Global E-bike Market:

[http://www.insg.org/%5Cdocs%5CINSG\\_Insight\\_23\\_Global\\_Ebike\\_Market.pdf](http://www.insg.org/%5Cdocs%5CINSG_Insight_23_Global_Ebike_Market.pdf) and

Wikipedia:

[https://en.wikipedia.org/wiki/Electric\\_bicycle\\_laws](https://en.wikipedia.org/wiki/Electric_bicycle_laws)

#### *Fitness trackers*

Fitness trackers are becoming an increasingly popular way for people to monitor how far they have walked or cycled, as well as information about heart rate, kilojoules burned, sleep patterns, etc. They are seen as a tool to both monitor and encourage participation in physical activity. Many fitness trackers come as a band or a watch worn on



the wrist.

Photo: Fitbit Alta Fitness Tracker on Harvey Norman website:

<http://www.harveynorman.com.au/fitbit-alta-fitness-tracker-black-large.html?CAWELAID=720013240000244636&gclid=CN722uLZ1swCFU9xvAodWAEIKg&clsrc=aw.ds>

*Charity events*

Charity walking, running and cycling events promoted by health related organisations to raise funds to fight cancer, diabetes, etc or by other organisations such as RSPCA to raise money for animal welfare have become increasingly popular.

*Technology*



Technology is constantly evolving and creating positive changes to the walking and cycling experience. One example is the "cyclee" which has been designed by Elnur Babayev to prevent the risk of accidents for cyclists. He created a device, attachable behind the bike's saddle and equipped with captors, able to determine whether the cyclist has stop, is about to turn or about to ride and to project the adequate sign on his / her back.

Source: <http://www.mobility-trends.com/index.php/2013/02/customize-ones-bike-how-bicycle-is-becoming-a-personalized-object>

Another example is the Walk with Path's *Path Finder* which aims to tackle 'freezing of gait' (FoG) – a common challenge for people with Parkinson's disease. People with Parkinson's often feel as if their feet are glued to the floor. Falls are common and this significantly affects their confidence in attempting to walk. A solution to FoG is a pair of shoes with lasers at the front which project green lines ahead as the person walks, activated by a sensor in each heel that monitors the user's walking pattern and pace.

Path Feel, another product developed by Walk with Path is an insole that provides haptic (vibration) feedback to the user when s/he walks. The tactile cues created by the vibration to enhance sensory perception of the environment and help to trigger movement.

Source:



[http://www.designcouncil.org.uk/news-opinion/pathing-way-confident-walking-story-walk-path?utm\\_source=Design%20Council%20Newsletter&utm\\_campaign=8242852806-Newsletter\\_160203&utm\\_medium=email&utm\\_term=0\\_a2748d9827-8242852806-67059697](http://www.designcouncil.org.uk/news-opinion/pathing-way-confident-walking-story-walk-path?utm_source=Design%20Council%20Newsletter&utm_campaign=8242852806-Newsletter_160203&utm_medium=email&utm_term=0_a2748d9827-8242852806-67059697)

*Road modifications*



New ways of separating and protecting cyclists from motorists have been implemented in recent years, such as the Copenhagen lane in Melbourne (based on a Danish design).

Source: Bicycle Network:  
<https://www.bicyclenetwork.com.au/general/for-government-and-business/2845/>

*Lighting*

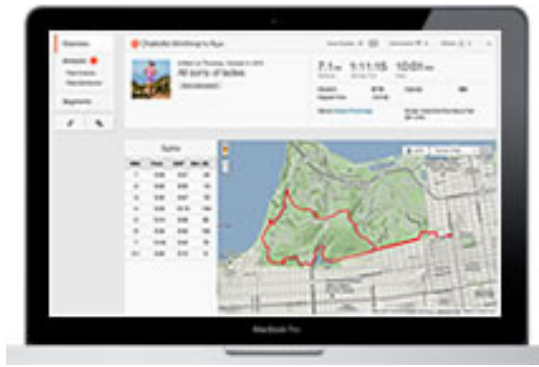
Installation of lighting along trails to encourage people to utilise trails early in the morning or in the evening is becoming more common.

*Electronic communication*

There is an expectation by user walkers, runners and cyclists that information about their preferred activities, i.e. maps, details of trail distances / gradients, availability of infrastructure, accommodation options, public transport options, trip reviews, etc is available on the internet and in the case of accommodation, is able to be booked online.

*Apps for smart phones and electronic devices*

There are numerous apps on the market that encourage people to walk, run and cycle. These apps allow people to map and record their route, record their times, compete against others, measure their heart rate, record their actions, etc. Examples include



Strava, Map my Walk, Just Start Walking, Runkeeper, Cycle Meter, Dirt School.

*Urban bike parks*



There is a growing trend in the US and internationally to develop urban bike parks featuring a wide variety of cycling opportunities such as toddler learn to ride areas, skill development trails, family trails, MTB trails, etc.

Source: Valmont Bike Park, City of Boulder, Colorado, USA

*Liveability*

There is a much greater understanding of the importance of pedestrian, cycling and public transport connections in making a place more liveable and the need for multi-modal transport planning. Walkable neighbourhoods are considered to be very important. Many cities around the world have banned cars from the centre of towns.

*Walking groups*

Formal walking groups have been established by organisations such as the Heart Foundation to help increase people's health and wellbeing.

*Active transport*

There is a much greater focus on planning for active transport (i.e. walking, cycling and public transport) in communities.

*Public art*

Increasingly art work is being installed as part of cycling trails. A glowing bicycle path inspired by Van Gogh's famous painting "The Starry night" was installed in Eindhoven, Netherlands as part of a collaboration between construction firm Heijmans and Dutch artist Daan Roosegaarde. The 600-meter long path is made of thousands of sparkling blue and green stones that can absorb solar energy during the day and emit light by night.





Source: <http://www.mobility-trends.com/index.php/2014/07/innovative-signage-for-enhanced-urban-mobility>

*Environmentally Sustainable Design*



Environmentally Sustainable Design features such as the SolaRoad in the Netherlands are starting to be constructed. This is a solar powered bike lane which will provide power to street lights and houses in Krommenie, a town northwest of Amsterdam. It is the result of a collaboration between the private sector, the government and academia. Although only 70m in length to begin with, this prototype will be expanded when funds become available. Source: <http://www.mobility-trends.com/index.php/2014/07/innovative-signage-for-enhanced-urban-mobility>

## 8.0 Community consultation

### 8.1 Key stakeholder workshop

A workshop was held at Central Goldfields Shire Council offices on 17 February 2016 with the following 16 individuals in order to identify key issues and opportunities:

Name	Organisation
Martin Mark	Central Goldfields Shire Council
Ben Robertson	Central Goldfields Shire Council
Ron Potter	Central Goldfields Shire Council
Diane Daniel	Central Goldfields Shire Council
Alisha Chadwick	Central Goldfields Shire Council
Jesse Sherwood	Sport and Recreation Victoria
Angela Daraxoglou	Department of Economic Development, Jobs, Transport and Resources
Adrian Dolley	Department of Economic Development, Jobs, Transport and Resources
Allan Carter	VicRoads
Ken Kao	VicRoads
Dean Steinberg	Bicycle Network
Dennis Farrell	Maryborough Police
Emma Shannon	Central Victorian Primary Care Partnership
Atrel Turner	Highview College
Phil Ashton	Dunolly cycle trails
Jim Field	Cycling enthusiast

Key issues and opportunities identified through these interviews include:

Issue	Details
Barriers	<ul style="list-style-type: none"> <li>• Rate capping by CGS limits funds available for construction and maintenance.</li> <li>• Ability to raise matching funding for grants is a challenge for Council.</li> <li>• Council reliant on external funds for many works.</li> <li>• Some people lack confidence to ride.</li> <li>• Rural communities have to compete with metropolitan areas for Government funding.</li> <li>• There is a negative attitude towards cyclists by some sectors of the community – need to counteract this.</li> <li>• There appears to be a lack of political will to fund cycling.</li> <li>• With events such as Veterans cycling, businesses are not providing things for partners and kids so they don't come back in future years.</li> </ul>
Benefits	<ul style="list-style-type: none"> <li>• Mental health benefits.</li> </ul>
Clubs / groups	<ul style="list-style-type: none"> <li>• Need a women's cycling group – women not as confident – great social benefits – leader training.</li> <li>• Previously there was a woman who ran mountain bike lessons for women.</li> <li>• Need a "club" for young people with mentors to get kids/youth to ride on weekends.</li> <li>• Dragon Cycles facilitates mountain bike groups on</li> </ul>



Issue	Details
	<p>Wednesdays and Saturdays.</p> <ul style="list-style-type: none"> <li>Road cycling groups – Tuesday and Thursday at 6.30am for fast riders, Wednesday morning and afternoon and Sunday for anyone.</li> <li>Groups are promoted via word of mouth.</li> <li>Social cycling groups tend to stay informal due to insurance issues.</li> <li>Local riders are happy to take people for a ride, but concerned about liability.</li> <li>Bicycle Network Victoria provides insurance for bike riders via membership and also provides public liability cover for one off events / rides for \$5.</li> <li>There are opportunities to develop clubs for on road and MTB cycling.</li> </ul>
Council's role	<ul style="list-style-type: none"> <li>Provide the place for walking and cycling.</li> <li>Who, apart from Council, has a role in facilitating programs?</li> </ul>
Cycle tourism	<ul style="list-style-type: none"> <li>Quantify value of cycle tourists in the region.</li> <li>Articulate benefits, e.g. quiet country roads.</li> <li>Identify income and employment outcomes of trails.</li> <li>Need supporting infrastructure to encourage cycling tourism e.g. bakeries.</li> </ul>
Cycling to school / work	<ul style="list-style-type: none"> <li>Need to encourage more kids to ride to school.</li> <li>The percentage of adults who ride to work has decreased over the years.</li> <li>Provide incentives for students e.g. certificate for riding/walking to school for 'x' number of days or km's – prizes etc.</li> <li>Engage with Ride to School Program (coordinated by Bicycle Network) – there are lots of free resources and advice.</li> <li>Encourage and support Bike Ed in all schools.</li> <li>Identify and engage with leaders at schools to lead cultural change to walking and riding.</li> <li>Bike SA sends volunteers out into schools – consider this as a potential model.</li> </ul>
Economics	<ul style="list-style-type: none"> <li>Articulate the economic benefits of walking and cycling, e.g. increase retail spend if it is easier to access retail area.</li> <li>Identify cost of physical inactivity.</li> </ul>
Education	<ul style="list-style-type: none"> <li>Need to educate cyclists and motorists about road rules.</li> <li>State schools have access to bike safety funding, but not Highview College.</li> <li>VicRoads has a trailer with bikes and hires it out to schools and communities.</li> <li>Council would like to purchase its own trailer.</li> <li>Need better relationship between cyclists and drivers re: safety. Need to promote via media.</li> </ul>
Enablers	<ul style="list-style-type: none"> <li>Need to build culture of cycling.</li> <li>Need to find what motivates people to cycle and build on that.</li> </ul>

Issue	Details
	<ul style="list-style-type: none"> <li>• There are low traffic volumes in the Shire.</li> <li>• There are some good on road rides available.</li> <li>• The climate is conducive to cycling.</li> <li>• There is a train that services Maryborough from Ballarat.</li> <li>• Need to create a broad regional development perspective around cycling walking as the North East of Victoria has done.</li> <li>• Community and businesses need to be 'pro bike' and offer quality customer service.</li> </ul>
Environmental influences	<ul style="list-style-type: none"> <li>• Cycling and walking in the natural environment relieves stress.</li> </ul>
Data	<ul style="list-style-type: none"> <li>• There is a gap in quality data about economic impact of cycling.</li> <li>• Need to do long term data gathering.</li> </ul>
Footpaths	<ul style="list-style-type: none"> <li>• The design of footpaths is important in order to encourage active transport by older adults.</li> </ul>
Infrastructure	<ul style="list-style-type: none"> <li>• Council needs to ensure regulations are met by developers / businesses re: bike shelters, showers, etc.</li> <li>• Ensure that schools and larger organisations have bike storage.</li> </ul>
Maintenance and renewal	<ul style="list-style-type: none"> <li>• Need to increase maintenance and renewal of tracks and infrastructure.</li> </ul>
Mountain bike riding (MTB)	<ul style="list-style-type: none"> <li>• There are some unofficial MTB tracks – would like to formally develop these in partnership with Parks Victoria.</li> <li>• Develop a MTB park.</li> <li>• Map MTB tracks.</li> </ul>
Partners	<ul style="list-style-type: none"> <li>• Potential partners include: Rotary, CFA, Zonta, grandparents (for school based programs such as Walking School Bus), Physical Activity Group.</li> </ul>
Planning scheme	<ul style="list-style-type: none"> <li>• Need to strengthen the local planning scheme to ensure that adequate paths are constructed.</li> </ul>
Programs	<ul style="list-style-type: none"> <li>• Need to re-introduce walking school bus.</li> <li>• Pool does 1000 lap club – could do the same with cycling and walking. Have tally board in cafes, e.g. Goldfields Res (has a café).</li> <li>• Council could facilitate 'Come 'n' Try' days for walking and cycling.</li> <li>• The indoor leisure centre in Maryborough conducts spin classes – this is a safe step towards cycling on shared paths or roads.</li> <li>• Ride to Work and School Day worked well – need to run these programs again.</li> <li>• Consider running a cycling boot camp.</li> <li>• Consider running programs to introduce people to cycling.</li> <li>• Encourage young people involved in St Luke's and DHHS to take part in a cycling event.</li> <li>• Organise long weekend bike events and ensure that there are day rides and loops of varying lengths</li> </ul>

Issue	Details
	<p>incorporated into the event.</p> <ul style="list-style-type: none"> <li>• Consider facilitating cycling training camps.</li> <li>• Consider running a mentoring program for beginner cyclists.</li> <li>• Introduce Ride to Work once a week/month instead of once a year.</li> </ul>
Promotion	<ul style="list-style-type: none"> <li>• Need to better promote all types of cycling via internet and social media.</li> <li>• Need to get a positive message out to the community about the benefits of cycling.</li> <li>• Promote walking tracks (Paddy's Ranges).</li> <li>• The Bicycle Advisory Group (BAG) should promote its successes and outcomes to the community.</li> <li>• Should advertise events through Bicycle Network.</li> </ul>
Public transport	<ul style="list-style-type: none"> <li>• The ability to transport bikes on trains is at the discretion of conductor – big issue – particularly with bus substitutions.</li> </ul>
Signage	<ul style="list-style-type: none"> <li>• Need to provide signs to state which paths are shared and the rules of use.</li> </ul>
State Government agencies	<ul style="list-style-type: none"> <li>• Active Transport Victoria being established in July by State Government.</li> <li>• Victoria's Cycling Strategy is being updated.</li> <li>• VicRoads has identified projects around State – including a loop in Maryborough.</li> <li>• State Govt and Transport Accident Commission (TAC) Road Safety Strategy – working towards zero deaths.</li> <li>• SA has brought in a law that cars need to be 1.5m from cyclists.</li> <li>• Driving test – perhaps need questions around cycle safety.</li> <li>• VicRoads reviewing cycling rules, e.g. footpath riding for people over 12 years of age.</li> <li>• Vic Cycle Strategy should be released mid-year.</li> <li>• Council has a good relationship with VicRoads, DELWP and Parks Victoria, but needs to engage more effectively with the health sector and the Department of Education.</li> <li>• There has been a lack of communication between Bicycle Network and the Shire in the past.</li> <li>• Discuss with Transport Accident Commission (TAC) the Government's \$100M commitment.</li> </ul>
Strategy	<ul style="list-style-type: none"> <li>• State that physical inactivity is one of the key reasons to provide paths/trails.</li> <li>• Need to have a focus on physically inactive people.</li> <li>• Cycle Central Goldfields – build the brand.</li> <li>• Don't over-invest in niche markets such as the velodrome; instead, focus on increasing everyday cycling to shops.</li> <li>• Need the community to drive walking and cycling initiatives as Council does not have the resources to do everything.</li> <li>• Need to focus on a strategic investment in the off-road</li> </ul>

Issue	Details
	<p>network. Where can we get best bang for our buck?</p> <ul style="list-style-type: none"> <li>• Need to integrate transport – buses/trains, cycling and walking.</li> </ul>
Technology	<ul style="list-style-type: none"> <li>• E-bikes may encourage more people to cycle.</li> <li>• Strava and other apps are becoming increasingly popular.</li> </ul>
Velodrome	<ul style="list-style-type: none"> <li>• Promote junior cycling.</li> <li>• Need to increase use of the velodrome.</li> </ul>
Works required	<ul style="list-style-type: none"> <li>• Road shoulders need to be widened and roads graded, e.g. Talbot needs an apron (Ballarat – Maryborough Rd – London Bridge).</li> <li>• Explore development of paths along reserve areas (i.e. main drain) in Maryborough (away from vehicles) to provide access to work, schools, shops.</li> <li>• Explore rail trail opportunities, e.g. Maryborough to Castlemaine.</li> <li>• Consider installation of outdoor exercise equipment around Lake Victoria.</li> <li>• Park Road – Golf Course to Gardiner Crt in Maryborough needs a path.</li> <li>• Need to fill in missing links along the route to Highview College.</li> <li>• Consider developing other recreational routes through the town, e.g. Goldfields Reservoir.</li> <li>• Consider extending the on road Ballarat – Maryborough Heritage Trail to Bendigo or Ararat.</li> <li>• Need a better on road path from Maryborough to Dunolly.</li> <li>• Maryborough/Avoca road needs new shoulders.</li> <li>• Ballarat/Maryborough road needs new shoulders.</li> <li>• London Bridge needs an upgrade (cycle path on the side)</li> <li>• Upgrade Dunolly cycling tracks.</li> <li>• Dunolly trails need to be rated and some more diversity of trail experience offered.</li> </ul>

## 8.2 Key stakeholder interviews

Interviews were conducted either face to face or via telephone with the following 34 individuals in order to identify key issues and opportunities:

Name	Organisation
Martin Mark	Central Goldfields Shire Council
Ben Robertson	Central Goldfields Shire Council
Mark Johnson	Central Goldfields Shire Council
Wayne Belcher	Central Goldfields Shire Council
Ron Potter	Central Goldfields Shire Council
Diane Daniell	Central Goldfields Shire Council
Alisha Chadwick	Central Goldfields Shire Council
Joel Chadwick	Central Goldfields Shire Council
Miriam Smith	Central Goldfields Shire Council
David Sutcliffe	Central Goldfields Shire Council
John Anthony	Central Goldfields Shire Council
Wally Cron	Central Goldfields Shire Council
Gillian Tattersal	Central Goldfields Shire Council
Brendan Regan	Central Goldfields Shire Council
Megan Clifford	Healthy Together
Sharyn Huggett	Healthy Together
Ross Forster	Maryborough businesses / cycling enthusiasts
Jim Field	Maryborough businesses / cycling enthusiasts
Bronwen Haywood	Maryborough businesses / cycling enthusiasts
Sharon Hiley	Dunolly Neighbourhood House
Sharon Fraser	Go Goldfields
Matthew Broad	Go Goldfields
Rayoni Nelson	VicHealth
Dean Steinberg	Bicycle Network
Atrel Turner	Highview College and Bicycle Advisory Group
David Sutton	Maryborough Education Centre
Jesse Sherwood	Sport and Recreation Victoria
Shelley Mulqueen	Sports Focus
Di Nevitt	VicRoads
Dennis Farrell	Maryborough Police
Sharon Finch	Central Highlands Water
Audrey Bartlett	Youth Advisory Council Coordinator
Charles Ablitt	Bushwalking Victoria
Tom Finning	Veterans Cycling Classic

Key issues and opportunities identified through these interviews include:

Issue	Details
Active Transport Victoria	<ul style="list-style-type: none"> <li>State Government has developed a new division within the Department of Economic Development, Jobs, Transport and Resources called Active Transport Victoria which starts on 1 July 2016. It will provide a coordinating public point of contact to lead the government's active transport policy, programs and infrastructure.</li> <li>The State Government is also rewriting Victoria's Cycling Strategy.</li> </ul>

Issue	Details
Ballarat to Maryborough Heritage Trail	<ul style="list-style-type: none"> <li>The Ballarat to Maryborough Heritage Trail is a key project for the municipality. The Shire is developing signage and a brochure.</li> </ul>
Barriers	<ul style="list-style-type: none"> <li>Age, fitness, people wary of injuring themselves, laziness, people wanting to be an expert before they have built up fitness / skill levels, it's easier to drive and park.</li> <li>There are negative messages about the dangers of walking and cycling.</li> <li>Requirements of work may impact on whether or not someone feels able to walk / ride to work, i.e. have to drive in a car which is used by others, have to carry laptop computers</li> <li>Workplaces not geared up to cyclists, e.g. no showers, no safe storage area for bikes, nowhere to iron clothes, etc.</li> <li>Council fear of litigation for facilities like dirt jumps.</li> <li>Not having the right type of bike.</li> <li>Don't know how to ride a bike.</li> <li>Many young people are studying and working at a part time job; leaving little time for cycling.</li> </ul>
Benefits	<ul style="list-style-type: none"> <li>People need to be made more aware of the benefits of walking and cycling – health, wellbeing, kilojoules burned, etc.</li> <li>Walking and cycling need to be seen as part of Council's economic development strategy as people who use active transport to town centres are more likely to spend more than those who drive.</li> </ul>
Bicycle hire	<ul style="list-style-type: none"> <li>Consider encouraging an existing business to provide bike hire services if there is sufficient demand.</li> </ul>
Bike racks	<ul style="list-style-type: none"> <li>Need bike racks in front of Dunolly Bakery and outside the resource centre (need to have a heritage design).</li> <li>Need a bike rack at Maryborough Swimming Pool.</li> </ul>
BMX	<ul style="list-style-type: none"> <li>Explore potential to develop some BMX jumps next to the skate park.</li> </ul>
Clubs / groups	<ul style="list-style-type: none"> <li>There are no formal mountain bike clubs in the Shire, however there is an informal group that rides on Thursdays.</li> <li>A cycle racing club is currently being established.</li> <li>Dunolly Cycling Group holds a ride once per month.</li> <li>Most groups of cyclists consist of middle aged men – the number of young people who ride as a group is negligible.</li> </ul>
Council's role	<ul style="list-style-type: none"> <li>Encourage people to walk and cycle (rather than physically undertaking hands on work with Energy Breakthrough)</li> <li>Build the capacity of community groups to promote walking and cycling and support them to do so.</li> <li>Building the social enabling environment – including the development of partnerships.</li> <li>Provide the physical environment for walking and cycling</li> </ul>

Issue	Details
Cycling / walking tourism	<ul style="list-style-type: none"> <li>• Promote walking and cycling infrastructure and events</li> <li>• Need to focus on increasing cycle and walking tourism as the roads are quiet and there are not many hills.</li> <li>• Learn from North East Victoria's success in developing cycle tourism.</li> <li>• Need cafes / bakeries, etc open on weekends in order to encourage cycle tourism. Talbot, Clunes and Avoca have places open. Some venues now open in Maryborough on weekends, but need to get this information out to potential cycle tourists.</li> </ul>
Cycling and walking to school	<ul style="list-style-type: none"> <li>• Need to increase the number of children who cycle and walk to school.</li> <li>• Consider making the safe routes to school highly obvious, e.g. paint trails or install bollards to show the way.</li> <li>• Encourage parents to ride with their children to school.</li> <li>• Need shared paths to schools.</li> <li>• Bicycle Network's Active Paths Programs involves working with children in schools to plan safe routes – includes drop offs some distance from schools so children who live too far from school can be dropped off and walk the remainder of the way to school.</li> <li>• Need a secure bike parking area at school.</li> <li>• A new bike shed is being constructed at Highview College.</li> <li>• Need to find cost effective ways of operating Bike Ed in schools.</li> <li>• Number of children currently walking / riding all of the way or part of the way to school is not known.</li> <li>• Schools have found that it is too hard to fit Bike Ed into the curriculum.</li> <li>• A fleet of bikes at school is hard to maintain.</li> <li>• MEC has a route along Kars St and via the bush which is considered safe from traffic, with two manned crossings on Balaclava St and Gladstone St.</li> </ul>
Data collection	<ul style="list-style-type: none"> <li>• There are currently two trail counters on Maryborough – Carisbrook Trail, but it doesn't differentiate between cyclists and walkers. Others will be placed on Park Road near Garden Court and on Derby Drive near Rinaldi Drive.</li> <li>• There is a need for more data collection and more counters.</li> </ul>
Developer guidelines	<ul style="list-style-type: none"> <li>• There are no guidelines that specify the number of footpaths required on each street. Need a policy which specifies this.</li> </ul>
Education	<ul style="list-style-type: none"> <li>• Need to provide education programs for people so that they know how to wear a helmet, be correctly fitted to their bike, understand road rules, know how to repair a tyre, know how to use their gears and know how to ride in a group / pack. Particularly important before taking part in any event.</li> <li>• Hard to find competent people to run bicycle education</li> </ul>



Issue	Details
	<p>programs.</p> <ul style="list-style-type: none"> <li>• Would be good if a service club took on the role of educating motorists and truck drivers about cyclists at a national level (with advertisements in the media, etc)</li> <li>• Take education program to some of the larger employers in Maryborough, e.g. True Foods and the hospital to encourage greater participation in cycling.</li> <li>• Consider purchasing a bike education trailer – supported by Rotary, VicRoads, Victoria Police and potentially Bendigo Bank.</li> <li>• Car dooring is an issue on Inkerman and High Streets – may need an education campaign.</li> </ul>
Enablers	<ul style="list-style-type: none"> <li>• Fairly flat topography, things aren't far, there is a culture of walking in Maryborough.</li> <li>• The quality of footpaths, crossings, traffic calming devices, etc all influence whether or not people walk and cycle.</li> <li>• School crossing supervisors to allow children to safely cross the road.</li> </ul>
Events	<ul style="list-style-type: none"> <li>• Existing events include RACV Energy Breakthrough, Veterans Cycling events, triathlons, mountain bike orienteering events (state and national level), Audax events, Great Victorian Bike Ride, Spring into Summer Stroll, Relay for Life, Pyrenees Magic</li> <li>• Plans are in place to hold an annual cycling event on the eve of the AFL grand final – a 115km road ride and a shorter ride.</li> <li>• Would be good to see guided bicycle tours.</li> <li>• Council does not have the resources to run events.</li> <li>• Encourage the Herald Sun Tour organisers to hold a stage of the event in Central Goldfields Shire.</li> <li>• Encourage event organisers to hold regional events or qualifying events at the velodrome.</li> <li>• Veterans Cycling Event would like continued support from Council (e.g. in-kind support with vehicles to assist road closures as a minimum) to run its annual Easter cycling event.</li> <li>• Central Goldfields Shire is considered well positioned for cycling events as people can park their cars at their accommodation and ride directly to the starting point of most events.</li> <li>• There are limited things to do for people accompanying cyclists to events as shops are often closed during event times, e.g. Easter.</li> <li>• Need to attract more cyclists for events such as the Veterans Cycling Event – there used to be more competitors from interstate and some from New Zealand.</li> <li>• Support from Victoria Police is very positive for events.</li> </ul>
Footpaths	<ul style="list-style-type: none"> <li>• Consider reviewing gutters, touch pads and timing of traffic lights to make it easier for pedestrians.</li> <li>• There are audits of all of Council's footpaths.</li> </ul>



Issue	Details
	<ul style="list-style-type: none"> <li>• Need more footpaths in Dunolly (particularly on the back streets).</li> <li>• Council policy is for footpaths on one side of the street.</li> <li>• Need footpaths on both sides of the street.</li> <li>• Edith Cowan University study suggests that it is cost effective to have footpaths on both sides of the street.</li> <li>• There may be justification to provide a footpath on one side of the street only in a low use street such as a court</li> <li>• Some kerbs are too steep.</li> </ul>
Funding sources	<ul style="list-style-type: none"> <li>• VicRoads for priority cycling routes</li> <li>• Federal Government's Stronger Regions</li> <li>• Regional Development Victoria</li> </ul>
Hierarchy	<ul style="list-style-type: none"> <li>• Classify paths as per road hierarchies, e.g. collectors, distributors and local. Inkerman and Gladstone Streets in Maryborough are collectors for example.</li> </ul>
Infrastructure	<ul style="list-style-type: none"> <li>• Need to ensure that there is suitable infrastructure such as seats for walking trails on Parks Victoria land.</li> <li>• Bike racks at resource centre are hidden from view.</li> <li>• Careful consideration needs to be given to the placement of seats along trails.</li> <li>• Need bike racks at shopping centres.</li> <li>• Need more drinking fountains around towns.</li> </ul>
Local Government rate capping	<ul style="list-style-type: none"> <li>• Rate capping by Local Government Authorities may have an impact on what actions the strategy can afford to deliver.</li> </ul>
Maps	<ul style="list-style-type: none"> <li>• A map of trails in Maryborough has been added to the Dunolly Trails map.</li> <li>• Some off road cycling routes have been mapped out.</li> <li>• Cycling maps need to identify roads that should be avoided.</li> <li>• A variety of maps should be produced, i.e. commuter routes, road rides, mountain bike tracks, family rides.</li> <li>• Maps need to be made available on Council's website, at the Visitor Information Centre and on the Visit Maryborough website.</li> <li>• Should mark bus routes on Maryborough town cycling map so people can plan a trip using several forms of active transport if necessary.</li> </ul>
Maintenance	<ul style="list-style-type: none"> <li>• Dunolly MTB trails need to be better maintained.</li> <li>• Work for the Dole program and prisoners are a good option for some maintenance or capital work developments.</li> <li>• Consideration needs to be given to the timing of maintenance tasks such as watering next to trails.</li> </ul>
Management plans	<ul style="list-style-type: none"> <li>• There is no management plan for the Goldfields Track.</li> </ul>
Mountain bike riding	<ul style="list-style-type: none"> <li>• There are some good mountain bike riding areas in the bush around Maryborough, which are easy to access.</li> <li>• Consider developing a number of formal mountain bike trails (perhaps in a circuit) in the bush around Maryborough with links into the town. Will need support from Parks Victoria.</li> </ul>

Issue	Details
Neighbouring municipalities	<ul style="list-style-type: none"> <li>• Need to be aware of walking / cycling developments in neighbouring municipalities – particularly those near the municipal border. Also consider developing an on road route to the Moonambel wine district in Pyrenees Shire.</li> </ul>
On road routes	<ul style="list-style-type: none"> <li>• Need to map on road routes for training cyclists. Shire is well suited to on road riding due to the fact that most roads are relatively quiet.</li> </ul>
Partners	<ul style="list-style-type: none"> <li>• Partner with Heart Foundation.</li> </ul>
Physical Activity Group	<ul style="list-style-type: none"> <li>• The Physical Activity Group, headed up by Council, has a focus on walking and physical activity.</li> </ul>
Planning principles	<ul style="list-style-type: none"> <li>• Ensure that trails connect to schools, shops and parks.</li> <li>• Ensure that aged care facilities are linked to trails.</li> </ul>
Planning scheme	<ul style="list-style-type: none"> <li>• Footpaths and cycle paths in new residential developments required as part of the Planning Scheme – this needs to be enforced by Council and detail of what is required needs to be communicated to developers.</li> </ul>
Potential trail works	<ul style="list-style-type: none"> <li>• Carisbrook &gt; Tullaroop Road &gt; Bendigo Road</li> <li>• Park Road near the Golf Course – requires a footpath</li> <li>• Derby Road towards Goldfields Reservoir – need to upgrade bitumen walking track that crosses driveways</li> <li>• Develop a safe crossing point over Pyrenees Highway near Bristol Hill</li> <li>• Identify preferred priority cycling routes for each town – a north-south route and an east-west route.</li> <li>• Identify a principal pedestrian network for Maryborough – one route into town and one route to Maryborough Education Centre.</li> <li>• Need to have a marked, safe bike route (potentially painted) from Maryborough East to Maryborough Education Centre (MEC) to encourage students to ride to school.</li> <li>• Need wider shoulders on many roads.</li> <li>• There needs to be a shoulder on the Maryborough to Avoca Road.</li> <li>• Extend path on Gladstone St to Phelans Rd and back along Avoca Road</li> <li>• Extend the training area for Energy Breakthrough into Lake Victoria camping area.</li> <li>• Need a path along Tullaroop road</li> <li>• Upgrade rail crossing on Derby Road (hopeful that Vic Track will cover the cost of this if it reinstates rail line to Ararat.</li> <li>• Develop a circular cycling route around Maryborough.</li> <li>• Consider a trail from Bealiba to the Reservoir.</li> <li>• Need a pedestrian crossing with lights between Cramer and MKM – may need to relocate bus stop slightly.</li> <li>• Consider developing a trail from Maryborough to Castlemaine via Carisbrook, Newstead, Baringhup and Maldon along rail line.</li> </ul>

Issue	Details
	<ul style="list-style-type: none"> <li>• Need an off road area for Energy Breakthrough participants to train, e.g. Hedges Oval, Carisbrook Recreation Reserve or Maryborough to Carisbrook Trail.</li> <li>• The corner of Balaclava Road and Inkerman St is considered a dangerous location by schools.</li> <li>• Need a shared path from Gladstone St in the west around Phelan's Road for school children to travel safely to MEC.</li> <li>• Highview College needs safe north, south, east and west routes. Need a path along Kars St (which could also link to MEC). There is some concern about passive surveillance in the Kars St bushland.</li> <li>• Need a trail to cross from Derby St to Highview College (Churchill St / Marrons Reef Road).</li> <li>• A trail from Park St, left into Tullaroop, right into Burns St and on to Highview would be good.</li> <li>• Would like a shared trail on Inkerman Street near the Early Learning Centre.</li> <li>• Need a footpath along Kars St from High Street.</li> <li>• There needs to be shoulders on the roads at Daisy Hill.</li> <li>• Ensure that pedestrian crossings are linked to bus stops – particularly Napier St and Inkerman St.</li> <li>• Develop a trail that links Goldfields Reservoir, Maryborough Airport, Lake Victoria and Bristol Hill.</li> <li>• Young people would like dirt jumps to be reinstated, and would like to be involved in the development of the jumps.</li> <li>• Develop mountain bike trails in the bush.</li> <li>• Bushwalking Victoria is keen to work with Parks Victoria to mark out and sign post additional walking tracks. They are also keen to work in partnership with cycling groups.</li> </ul>
Programs	<ul style="list-style-type: none"> <li>• Consider running cycling programs in the school holidays.</li> <li>• Dunolly Neighbourhood House runs a walking group at 9.30am on Mondays – approximately six participants.</li> <li>• Potentially a demand for a buddy program for cycling.</li> <li>• The paperwork for the Heart Foundation Walks was considered too onerous for the community.</li> <li>• Consider the Active Script program, or a version of it.</li> <li>• Dunolly Neighbourhood House will consider offering a shorter half hour walk for beginner walkers.</li> <li>• Bicycle education programs are important.</li> <li>• Would like to re-initiate the bicycle recycling program whereby local residents donated unused bicycles and young people were taught how to fix them up and maintain them, and to ultimately keep the bicycle.</li> <li>• Annual walk to school day held in October – funded by VicHealth.</li> <li>• Schools don't believe that they have the resources to</li> </ul>

Issue	Details
	<p>fund walk or cycle to school days – want Council to facilitate.</p> <ul style="list-style-type: none"> <li>• Premier’s Active April can be used by Councils to promote cycling and walking.</li> <li>• Consider re-introducing Walking School Bus and Cycling School Bus programs.</li> <li>• Perhaps start a kilometre clubs in schools and the general community.</li> <li>• Consider running regular community rides with a social focus that can be completed in 60 minutes.</li> <li>• Promote Ride to Work Day.</li> <li>• Consider promoting a program being trialled in Bendigo whereby people are encouraged to use active transport to work a minimum of one day out of every five.</li> <li>• Consider establishing a women’s riding group.</li> <li>• Schools are concerned about liability of walking and cycling events.</li> <li>• Maryborough Police would be an ideal partner in Bike Ed programs.</li> <li>• Teachers need to role model active transport.</li> <li>• Consider introducing programs like those in Ararat (e.g. Activate Ararat – mentor and champion model) which were introduced following the Biggest Loser TV show.</li> <li>• Need to develop a program that encourages anyone who is travelling two or less kilometres to use active transport instead of the car.</li> <li>• Investigate the British Cycle to Work scheme whereby the government introduced an annual tax exemption, which allows employers to loan cycles and cyclists’ safety equipment to employees as a tax-free benefit.</li> <li>• Consider building a festival around the Energy Breakthrough – a design your own bike competition and walking / cycling tours for supporting teachers and parents. Acknowledge that Council resources are an issue though.</li> <li>• Consider starting up a Park Run program.</li> <li>• Encourage and promote geo-caching.</li> <li>• Health centre in Maryborough operates the Active Script program.</li> <li>• Develop a model like the Macedon Ranges Running Club to encourage more cyclists, walkers and runners in Central Goldfields Shire.</li> <li>• Explore what role Neighbourhood Houses should have in the promotion and facilitation of walking and cycling initiatives.</li> <li>• Need place based programs and infrastructure – particularly important in low socio economic areas.</li> <li>• Daniel Davies from Dragon Cycles runs a program (on a weekly basis for a term) in conjunction with students in years 7-9 to learn how to maintain bicycles and to</li> </ul>

Issue	Details
	go mountain bike riding.
Promotion	<ul style="list-style-type: none"> <li>• Staff at the Visitor Information Centre need to be provided with information about walking and cycling opportunities so that they can share this information with visitors.</li> <li>• There is a lack of information available about where to walk and cycle in the Shire.</li> <li>• Need to promote safe cycling routes and also inform potential cyclists about routes that are not as suitable for riding.</li> <li>• Market Central Goldfields Shire as a cycling centre (e.g. Cycle Central Goldfields).</li> <li>• Council needs to promote when it has upgraded trails – i.e. keep trails at the forefront of people’s minds.</li> <li>• Trails in the Shire need to be better promoted via websites.</li> <li>• Include photos of people walking or cycling in Council’s newsletters and on its website on a regular basis – and link this back to the Walking and Cycling Strategy.</li> <li>• Use social media tools to promote trails – instagram, facebook, etc.</li> <li>• Recognise that IT literacy and connectivity in the Shire is low, but mobile phone ownership is high, when considering promotional campaigns.</li> </ul>
Prioritisation	<ul style="list-style-type: none"> <li>• How will trail upgrades be prioritised? In streetscape strategy, roads with the highest route significance and vehicle count are given the highest priority. Other roads require petitions from residents in the street.</li> </ul>
Public transport	<ul style="list-style-type: none"> <li>• Re-opening of passenger rail services between Ballarat and Maryborough has provided an opportunity for people to travel by train to Maryborough and Talbot, however, the train timetables don’t necessarily suit cyclists (i.e. cyclists need to stay one or two nights).</li> <li>• There are buses between Ballarat and Maryborough but no guarantee that bikes can be carried in the storage area.</li> <li>• Public transport routes need to be connected to cycling / walking trails and bike racks.</li> </ul>
Rail trails	<ul style="list-style-type: none"> <li>• The railway line between Maryborough to Ararat is likely to reopen, so this will not be an option for a rail trail.</li> <li>• There may be an opportunity to establish a rail trail from Maryborough to Castlemaine via Newstead, although there is a push to have passenger rail reinstated on this line.</li> </ul>
Resources	<ul style="list-style-type: none"> <li>• An active transport officer, shared between several Councils, could be useful.</li> </ul>
Scooters	<ul style="list-style-type: none"> <li>• There are a significant number of people riding scooters in the municipality.</li> <li>• Should identify safe routes for scooters into the CBD.</li> </ul>

Issue	Details
	<p>Route should include gradients of streets and crossing points.</p> <ul style="list-style-type: none"> <li>• Need a safe scooter route from Havilah Aged Care Facility, Holyrood St and Park Road.</li> </ul>
Signage	<ul style="list-style-type: none"> <li>• More signs warning motorists of cyclists need to be placed on the back road from Maryborough to Dunolly.</li> <li>• Need to review signage on all routes and budget for replacement of signs.</li> <li>• Need signs on both directions on the Dunolly Trails.</li> <li>• Consider establish a walking track in Bealiba with interpretive signage.</li> <li>• Need improved signage along trails managed by Parks Victoria.</li> <li>• Upgrade pedestrian signs around the municipality.</li> <li>• Need signs in towns to welcome cyclists.</li> </ul>
Strategy development	<ul style="list-style-type: none"> <li>• Need to link the strategy to other Council documents. Need to incorporate an evaluation framework. Strategy is for 10 years and should feature short, medium and long term goals. It should be aspirational, but realistic.</li> <li>• Need to have a target about increasing the number of people who walk and cycle.</li> <li>• Need to identify the key success factors for trails, e.g. accessibility, distances, food / drinks</li> <li>• Need to better activate existing trails</li> <li>• Council's Municipal Health and Wellbeing Plan must be the basis for all that Council does.</li> </ul>
Technology	<ul style="list-style-type: none"> <li>• Consider the role of apps and Strava in promoting cycling and cycle tourism.</li> </ul>
Trail design	<ul style="list-style-type: none"> <li>• Some existing trails may need to be wider. Preference is for 3m wide trails. Most footpaths are only 1.5m wide. Road reserve in Maryborough and Talbot is only 20 metres, hence there is not a lot of space to develop shared paths. Road reserves in Carisbrook and Dunolly are 30 metres.</li> <li>• Would like to see a line marked down any path that is designed for shared use.</li> <li>• Shared paths are okay providing that traffic volumes are not high.</li> <li>• Shared paths may discourage older people from walking.</li> <li>• Bushwalkers prefer their own paths due to the potential speed of mountain bike riders in the bush, but if not possible, they prefer wider paths and a path off to the side in some spots where it's not possible to have a wider path.</li> <li>• Need to ensure that there is pedestrian access through car parks.</li> <li>• Careful consideration needs to be given to plantings near walking / cycling paths.</li> <li>• Need to ensure that trails / paths reflect the character of the town in which they are installed. Granitic sand is</li> </ul>

Issue	Details
	<p>likely to better meet the heritage look of some towns.</p> <ul style="list-style-type: none"> <li>• Granitic sand surface is fine for a recreational trail around town and is a cheaper surface than concrete or bitumen.</li> <li>• Trail surfaces around schools and aged care facilities should be sealed.</li> <li>• Sealing of shoulders on roads is important.</li> </ul>
Trail surface	<ul style="list-style-type: none"> <li>• Need to consider the type of seal used on roads – the coarse aggregate used on most roads is unsuitable for bike riders.</li> <li>• The edges of roads are currently in poor condition and haven't been graded for some time.</li> <li>• There are loose bits of old bitumen on the Maryborough to Ballarat Road – this is a hazard to bike riders.</li> </ul>
Training facility	<ul style="list-style-type: none"> <li>• Potential to establish a training facility at Bristol Hill and the velodrome for all types of cycling. Consider a skills course and BMX tracks.</li> <li>• Consider a skills training facility at the old adventure playground site – a closed circuit which is also suitable for young children to learn to ride.</li> </ul>
Trends	<ul style="list-style-type: none"> <li>• Fitness trackers such as fitbit</li> <li>• E-bikes</li> <li>• Charity events using cycling as a tool to engage participants</li> <li>• Trails being built by private organisations on their own land, e.g. 2.5km mountain bike trail at Bress Winery.</li> </ul>
Velodrome	<ul style="list-style-type: none"> <li>• The Maryborough Velodrome is in good condition but is under-utilised.</li> <li>• Potential to develop accommodation on site for training camps or school camps.</li> <li>• Need to seal the entrance road and improve drainage.</li> <li>• Car club uses the club house.</li> <li>• Parks Victoria owns a section of the velodrome at the rear and Council owns a section at the front (consider a land swap).</li> </ul>



### 8.3 Community workshops and listening posts

Community workshops and listening posts were held in various towns throughout Central Goldfields Shire in March 2016 in order to identify key issues and opportunities related to walking and cycling. The total number of attendees was 68 – Bealiba – public meeting at the hall (8), Carisbrook - public meeting at the Senior Citizens Hall (5), Dunolly – listening post in front of the Dunolly Bakery (18), Maryborough – listening post in front of the Maryborough Newsagency (37) and Talbot – public meeting at the town hall (0). Key issues and opportunities identified include:

Issue	Details
Bealiba	<ul style="list-style-type: none"> <li>• Desire for a shared, compact gravel path in Bealiba</li> <li>• Develop a path to the cemetery from the centre of Bealiba</li> <li>• Concern in Bealiba re speeding cars – need to install more signs to show that there are children on bikes</li> <li>• Bike racks needed outside the Bealiba Hotel</li> <li>• Bealiba community has submitted a funding application to develop a technological interpretation trail (requiring 3D goggles)</li> </ul>
Bicycle Education	<ul style="list-style-type: none"> <li>• Bicycle Education programs are limited in schools due to lack of volunteers and people with appropriate skills.</li> <li>• Need to educate motorists re cyclists.</li> </ul>
Carisbrook	<ul style="list-style-type: none"> <li>• Need a link to the recreation reserve and Market Square.</li> <li>• Violet Hill is a growth area in Carisbrook – it needs to be connected to the cycling / walking network.</li> <li>• Pedestrian bridge over Deep Creek not wide enough for pedestrians and cyclists – suggest a detour along McCallum St near the car park and toilet over to Green St where the bridge is wider.</li> <li>• There is not enough space on the pedestrian walk on the Pyrenees Highway.</li> <li>• Extension to trail needs to go up Church Street or Smith Street to the Recreation Reserve and then down Birch Street to connect to the primary school.</li> <li>• Carisbrook – some streets have no footpaths – need them on at least one side of every street.</li> <li>• Need a footpath in Bucknall Street.</li> <li>• Need a footpath from the units at the corner of McLachlan and Urquart Streets – currently residents walk on the road.</li> <li>• There are trails at Dowie Reserve that are managed by ageing volunteers. This arrangement may not be sustainable long-term.</li> <li>• Bindis are a problem in Market Reserve in Carisbrook.</li> </ul>
Connecting towns	<ul style="list-style-type: none"> <li>• Would be good to connect towns by trails – Maryborough to Talbot, Dunolly and Bealiba.</li> </ul>
Cycling / walking to school	<ul style="list-style-type: none"> <li>• Need to identify safe routes to schools.</li> </ul>
Drinking water	<ul style="list-style-type: none"> <li>• Would ideally like drinking water available on off road tracks.</li> </ul>
Dunolly	<ul style="list-style-type: none"> <li>• Need to fix footpaths on side streets in Dunolly</li> <li>• Need shops open on weekends to attract cycle tourists</li> </ul>



Issue	Details
	<p>in Dunolly.</p> <ul style="list-style-type: none"> <li>• Need to upgrade Dunolly trails so that they are suitable for hybrid bikes.</li> <li>• Need off road trails around Dunolly</li> <li>• Need more signs on the Dunolly trails, particularly for people wishing to travel in the opposite direction to existing signage.</li> <li>• Bindii's are a problem in Dunolly – from the hospital to the school in particular.</li> <li>• Dunolly MTB trails are under-utilised.</li> <li>• Dunolly MTB trails need to be better promoted.</li> <li>• Develop a walking trail around the Dunolly Reservoir.</li> <li>• Develop an off road trail between Dunolly and Maryborough.</li> <li>• Continue to develop footpaths throughout the Shire like the ones in Dunolly.</li> <li>• Promote trails and events at the Dunolly Market.</li> <li>• Install an interpretive sign outside Dunolly Bakery outlining the town heritage walk.</li> </ul>
Eddington	<ul style="list-style-type: none"> <li>• Develop a trail from Eddington to Hogan's Bridge.</li> <li>• Lobby for speed limit on Eddington Bridge to be reduced to 80km per hour to make it safer for cyclists.</li> </ul>
Electric bikes	<ul style="list-style-type: none"> <li>• Consider providing charging stations (tend to get about 40km out of one charge).</li> </ul>
Energy Breakthrough Footpaths	<ul style="list-style-type: none"> <li>• A training track is needed for Energy Breakthrough</li> <li>• Need footpaths on at least one side of every street</li> <li>• Construct footpaths on both sides of the street.</li> <li>• Sensor solar lighting on streets could encourage more people to walk.</li> <li>• Need to better maintain footpaths.</li> </ul>
Infrastructure	<ul style="list-style-type: none"> <li>• Make sure there are seats along trails and as much shade as possible.</li> </ul>
Maps	<ul style="list-style-type: none"> <li>• Carisbrook community is considering having a town walking map printed on bakery bags, directing people to historic markers that exist around the town.</li> <li>• Desire for downloadable walking / cycling maps for each town.</li> <li>• Consider marking walking and cycling trails on town signs.</li> </ul>
Maryborough	<ul style="list-style-type: none"> <li>• At the Maryborough end of the Maryborough – Carisbrook Trail, trail users have to cross Tuaggra Street to get to the underpass. The trail stops at McDonalds. Cars park on both sides of Tuaggra Street. Would like to see the trail continue along Burns Street, onto Nolan Street and then finish at the Resource Centre in Alma Street where there is secure bike parking.</li> <li>• Seats on Paddy's Ranges walking track need replacing.</li> <li>• Train service doesn't work well for people wishing to access trails in the region via public transport, i.e. they need to stay one to two nights depending on their ride.</li> </ul>

Issue	Details
	<ul style="list-style-type: none"> <li>• Need lights on main crossings in Maryborough.</li> <li>• Need a footpath in Holyrood Street.</li> <li>• Curb is too high on west side of High Street Maryborough for scooters.</li> <li>• Footpath needs to be level, e.g. Napier St going up to IGA.</li> <li>• Develop an off road track between Maryborough and Daisy Hill.</li> <li>• Like the flat gutters in High St, Maryborough – install more of these.</li> <li>• Need a pedestrian crossing from the post office to the coffee shop.</li> <li>• Need footpaths to be constructed off Gladstone Street.</li> <li>• More dog excrement bags required throughout Maryborough parkland areas.</li> </ul>
Maintenance	<ul style="list-style-type: none"> <li>• Trail maintenance is important.</li> <li>• Council generally does a good job of maintaining the Maryborough – Carisbrook Trail.</li> <li>• Council has an app that the community can use to inform officers of areas requiring maintenance, linked to GPS coordinates</li> <li>• Trails need to be well maintained.</li> </ul>
Moliagul	<ul style="list-style-type: none"> <li>• Develop a trail around Moliagul.</li> </ul>
Prioritising projects	<ul style="list-style-type: none"> <li>• Undertake a cost benefit analysis prior to implementing initiatives.</li> </ul>
Programs	<ul style="list-style-type: none"> <li>• Promote geo-caching to encourage more people to walk or cycle.</li> <li>• Programs that rely on people having to upload or download information from computers limits participation in some areas</li> <li>• Consider Active Script program</li> <li>• May need to personally invite people to go walking / cycling in order to increase participation, e.g. Bring a Friend Day, Come 'n' Try Day.</li> <li>• Consider running a social dog walking group.</li> <li>• Shire should provide bikes to those who can't afford them.</li> <li>• Encourage Field Naturalists to conduct walks in spring.</li> <li>• Need more walk or ride to school days – perhaps one per month, with police involvement.</li> <li>• Need to run more programs / events to encourage more people to walk and cycle and to understand the benefits of active transport.</li> </ul>
Promotion	<ul style="list-style-type: none"> <li>• Need to promote all trails via Council's website, by having links to maps of the trail on the International Mountain Biking Australia (IMBA) website and Bicycle Network's website; have links on the Bendigo Regional Tourism website and the Dunolly Town website. Hard copy maps should be available at Dragon Cycles in Maryborough, at Visitor Information Centres and should also be distributed more widely outside the district.</li> </ul>

Issue	Details
Scooters	<ul style="list-style-type: none"> <li>• Better advertise cycling events.</li> <li>• Consider having charging stations for scooters in town and advertise the location of these charging stations.</li> <li>• Where there are no footpaths in streets, local residents who rely on scooters for their transport are reluctant to leave home.</li> <li>• Scooter drivers need to be educated about driving too fast.</li> </ul>
Signage	<ul style="list-style-type: none"> <li>• Need good signage on trails both within towns and within the bush.</li> </ul>
Trail design	<ul style="list-style-type: none"> <li>• Don't put dirt or rocks next to footpaths as people can slip over.</li> <li>• All footpaths should be shared use.</li> <li>• Trails need to be suitable for motorised scooters.</li> <li>• Trails need to link to destinations that people want to go to.</li> <li>• Widen roads to cater better for cyclists.</li> <li>• Need more shade trees along paths as well as seats.</li> <li>• Need more planters in the street to make walking more attractive.</li> <li>• Install fitness equipment along trails, e.g. pull up bar.</li> <li>• Place a line down the centre of shared paths so people know where to ride / walk.</li> <li>• Would like to see more off road trails.</li> <li>• Surface needs to be smooth.</li> <li>• Widen shoulders on country roads for cyclists.</li> </ul>

## 8.4 Cycling survey

A survey was developed in order to seek information from the general public about cycling issues and opportunities. Surveys were forwarded electronically to all of the organisations listed within Council's Community Directory. A total of 41 surveys were completed. 56% of surveys were completed by males and 44% by females. The majority of survey respondents are aged between 40-49 years of age (32%), followed by 30-39 year olds (22%), 60-69 year olds (15%) and 50-59 year olds (12%). The vast majority of surveys were completed by Maryborough residents (68%), followed by Carisbrook (12%), Bealiba (10%), Dunolly and Talbot (5%).

Key issues and opportunities identified include:

Issue	Details
Cycling frequency (41 responses)	<p>The majority of survey respondents cycle:</p> <ul style="list-style-type: none"> <li>• several times per week (41%)</li> <li>• daily (17%)</li> <li>• once per week (12%)</li> <li>• once every few months (12%).</li> </ul> <p>All survey respondents cycled at least once every few months.</p>
Type of cyclist (41 responses)	<p>Survey respondents describe themselves as a:</p> <ul style="list-style-type: none"> <li>• Fitness cyclist – long distance training (49%)</li> <li>• Recreational cyclist – riding on off road trails (39%)</li> <li>• Utility cyclist – for transport (29%)</li> <li>• Commuter cyclist – to work or school (24%)</li> <li>• Mountain bike rider – mountain bike trails (20%)</li> <li>• Cycle tourist – ride while on holidays (17%)</li> <li>• Competitive cyclist – take part in races (15%)</li> <li>• Touring cyclist – go on cycling holidays (10%)</li> </ul>
Number of bikes owned (40 responses)	<p>Approximately one third of survey respondents (35%) own two bikes, while 13 (32%) own one. 15% own three bikes, 13% four bikes and 5% own five bikes or more. All survey respondents own at least one bike.</p>
Where people ride (41 responses)	<p>The most popular destinations for riding, by those who completed the survey, are:</p> <ul style="list-style-type: none"> <li>• Just on the road for training (51%)</li> <li>• In the bush (39%)</li> <li>• To the shops (34%)</li> <li>• To a friend's house (29%)</li> <li>• To a lake / river (22%)</li> <li>• To a park / playground (22%)</li> <li>• On an exercise bike or in a spin class at gym (20%)</li> <li>• To work (20%)</li> </ul>
Which roads / tracks people use (34 responses)	<p>The roads / tracks that most survey respondents ride on include:</p> <p>General / municipal:</p>

Issue	Details
	<ul style="list-style-type: none"> <li>• Maryborough to Dunolly via Natte Yallock Rd - 3</li> <li>• Maryborough to Carisbrook - 3</li> <li>• Maryborough to Talbot - 3</li> <li>• Bike paths – 3</li> <li>• Maryborough to Maldon – 2</li> <li>• Maryborough to Timor – 2</li> <li>• Tullaroop Rd – 2</li> <li>• McCallums Creek Road section of Ballarat Road – 2</li> <li>• Public roads – 2</li> <li>• Bush tracks – 2</li> <li>• Any quiet roads away from traffic – 2</li> <li>• Maryborough to Avoca – 1</li> <li>• Maryborough to Clunes – 1</li> <li>• Maryborough to Dunoon via Timor – 1</li> <li>• Maryborough &gt;Daisy Hill&gt;Carisbrook&gt;Maryborough – 1</li> <li>• Maryborough&gt;Talbot&gt;Red Lion&gt;Craigie&gt;Carisbrook&gt;Maryborough – 1</li> <li>• Maryborough&gt;Timor&gt;Dunolly&gt;Betley&gt;Maryborough - 1</li> <li>• Maryborough&gt;Craigie&gt;Tullaroop Reservoir&gt;Carisbrook &gt;Maryborough – 1</li> <li>• Maryborough&gt;Mt Hooghly&gt;Daisy Hill&gt;Carisbrook - 1</li> <li>• Majorca Rd – 1</li> <li>• Landrigans Rd – 1</li> <li>• Timor Rd – 1</li> <li>• Whittles Rd – 1</li> <li>• Paddy’s Ranges – 1</li> <li>• Slaughter Rd Track – 1</li> <li>• Happy Tommy Track – 1</li> <li>• Avoca Rd - 1</li> <li>• Any roads in Bendigo area - 1</li> <li>• Rail trails - 1</li> <li>• Gravel driveway on own property - 1</li> </ul> <p>Bealiba:</p> <ul style="list-style-type: none"> <li>• Main St - 3</li> <li>• Cochrane St - 2</li> <li>• Grant St – 2</li> <li>• Davis St - 1</li> <li>• Cemetery Rd / Boundary Track – 1</li> <li>• Redgates Rd – 1</li> <li>• Mount Rd – 1</li> <li>• Orne St – 1</li> <li>• Rheola Rd – 1</li> <li>• Whitehead Lane - 1</li> <li>• Proctors Rd – 1</li> <li>• Queriples Rd – 1</li> <li>• Tin Pot Hill Track – 1</li> <li>• Logan Rd – 1</li> <li>• Scent Farm Rd - 1</li> </ul> <p>Carisbrook:</p> <ul style="list-style-type: none"> <li>• Crameri Rd - 1</li> </ul>

Issue	Details
	<ul style="list-style-type: none"> <li>• Carisbrook – Eddington Rd – 1</li> <li>• Chaplins Rd – 1</li> <li>• Bucknall St - 1</li> </ul> <p>Dunolly:</p> <ul style="list-style-type: none"> <li>• Dunolly Eddington Rd – 1</li> <li>• Dunolly Moliagul Rd – 1</li> </ul> <p>Maryborough:</p> <ul style="list-style-type: none"> <li>• Gladstone St – 3</li> <li>• Raglan St – 2</li> <li>• Energy Breakthrough Track – 2</li> <li>• Cemetery Rd – 2</li> <li>• Lake Victoria – 1</li> <li>• Goldfields Reservoir – 1</li> <li>• Dundas Rd – 1</li> <li>• Golf course Rd – 1</li> <li>• Newtown St – 1</li> <li>• Inkerman St – 1</li> <li>• Tuaggra St – 1</li> <li>• Burns St -1</li> <li>• Nolan St – 1</li> <li>• Clarendon St – 1</li> <li>• Park Rd – 1</li> <li>• Gillies St – 1</li> <li>• Sutton St - 1</li> <li>• Karsick St - 1</li> <li>• Hospital St - 1</li> </ul> <p>Talbot:</p> <ul style="list-style-type: none"> <li>• Talbot-Mt Grenoch Rd - 1</li> </ul>
<p>Cycling hours per week</p> <p>(40 responses)</p>	<p>In an average week, survey respondents were likely to ride:</p> <ul style="list-style-type: none"> <li>• 2-3 hours (38%)</li> <li>• 30 minutes (13%)</li> <li>• 6-7 hours (13%)</li> <li>• 1 hour (10%)</li> <li>• 4-5 hours (10%)</li> </ul>
<p>Enjoyable aspects of riding</p> <p>(41 responses)</p>	<p>Survey respondents identified the following factors that they most enjoy about riding:</p> <ul style="list-style-type: none"> <li>• It helps me to get fit / stay in shape (80%)</li> <li>• I enjoy being outside in the bush / on the open road (68%)</li> <li>• I enjoy the physical challenge (63%)</li> <li>• It's a good activity to do with friends / family (44%)</li> <li>• I know that it's good for the environment (non polluting form of transport) (44%)</li> </ul>
<p>Events / programs that people are interested in</p> <p>(40 responses)</p>	<p>Survey respondents identified the following types of cycling events / programs that are of most interest to them:</p> <ul style="list-style-type: none"> <li>• Local community or charity cycling events of 1-2 hours duration (43%)</li> <li>• Cycle tours to other areas (28%)</li> </ul>

Issue	Details
	<ul style="list-style-type: none"> <li>• Mass participation multi-day on road event, e.g. Great Victorian Bike Ride (25%)</li> <li>• Triathlons / ironman (23%)</li> <li>• Full day cycling events (18%)</li> <li>• Bike maintenance workshop (18%)</li> <li>• School based cycling programs, e.g. Ride to School Day (15%)</li> <li>• Cycling programs for women - increasing confidence, bicycle maintenance, etc. (13%)</li> <li>• Bike skills / mountain bike skills development course (10%)</li> </ul>
<p>Barriers to cycling (by those who already cycle)</p> <p>(36 responses)</p>	<p>Survey respondents identified the following factors as barriers that stop them from cycling more often:</p> <ul style="list-style-type: none"> <li>• I don't feel safe riding (39%)</li> <li>• There are no bike paths near my house (33%)</li> <li>• I don't have time (31%)</li> <li>• There's too much traffic on the roads (28%)</li> <li>• Because of the weather – it's often too hot, too cold or too wet (25%)</li> </ul>
<p>Enablers required to encourage people to cycle more often</p> <p>(33 responses)</p>	<p>Survey respondents identified the following enablers that would encourage them to cycle more often:</p> <ul style="list-style-type: none"> <li>• More off road trails – 5</li> <li>• Time – more time, better management of time – 5</li> <li>• Wider roads / shoulders – 3</li> <li>• More motivation – 3</li> <li>• Nothing – 3</li> <li>• Good quality paths – 2</li> <li>• Less work – 2</li> <li>• Prefer to do other activities as well – 1</li> <li>• Shoulder on the Maryborough to Dunolly Rd – 1</li> <li>• Off road trail next to Gladstone St in Maryborough so more children will be encouraged to ride to school – 1</li> <li>• Better infrastructure – pump stations, drink refill taps, proper bike stands – 1</li> <li>• Integration with public transport so both cyclists and their bikes can be Karsied on trains / buses – 1</li> <li>• More daylight hours – 1</li> <li>• Less housework - 1</li> <li>• Better road surfaces – 1</li> <li>• Roads / tracks suitable for mobility scooters – 1</li> <li>• Have people to ride with – 1</li> <li>• More cycling events in Victoria – 1</li> <li>• Details of local riding groups – 1</li> <li>• More cycle lanes in Maryborough CBD – 1</li> <li>• Cycle lanes leading into roundabouts – 1</li> <li>• More sealed local roads – 1</li> <li>• Removal of bindiis – 1</li> </ul>

Issue	Details
	<ul style="list-style-type: none"> <li>• More driver awareness – 1</li> <li>• Off-road trail from Crameri Rd / Williamson Crt to Carisbrook township - 1</li> </ul>
<p>Membership (17 responses)</p>	<p>Survey respondents identified any cycling related organisation that they are involved with:</p> <ul style="list-style-type: none"> <li>• Bicycle Network (47%)</li> <li>• Maryborough and District Triathlon Club (29%)</li> <li>• Informal group of friends who cycle (24%)</li> <li>• Central Victorian Veteran’s Cycling Club (24%)</li> </ul> <p>None of the survey respondents are members of International Mountain Bike Association (IMBA).</p>
<p>Works required on existing on-road lanes, off-road tracks or cycling facilities (25 responses)</p>	<p>Survey respondents believe that the following works are required to improve existing on-road lanes, off-road tracks or cycling facilities (each suggestion was made by one person only):</p> <p><b>Bealiba:</b></p> <ul style="list-style-type: none"> <li>• Cemetery Rd - improve surface and widen road to allow use by mobility scooters/walkers to safely use along with vehicles. Improve gravel path/road from Cemetery for access to grave sites - has uneven surface, rabbit holes and can be boggy in wet weather.</li> </ul> <p><b>Carisbrook:</b></p> <ul style="list-style-type: none"> <li>• Maryborough to Carisbrook Trail – surface is breaking apart in some areas. There is a dip in the middle of the track for a draw or power pit that makes it a bit dangerous to ride a bike on. Develop a loop by marking an on road lane on Bucknall St, Chaplins Rd, Tullaroop Rd and Burns St to extend this path. Carisbrook bike pathway has several sections where motor vehicles encroach.</li> </ul> <p><b>Dunolly:</b></p> <ul style="list-style-type: none"> <li>• Dunolly Rd</li> </ul> <p><b>Maryborough:</b></p> <ul style="list-style-type: none"> <li>• Paddy’s Ranges</li> <li>• Needs to be sealed access track/road to the velodrome as it is highly used and would promote more use of a great asset.</li> <li>• Derby Rd Track – upgrade and install a safe railway line crossing.</li> <li>• Track in the state forest at the end of Matthews Road, is very hard to ride on - uneven, large rivets/drops in road.</li> <li>• Better connect existing bike paths.</li> <li>• Pave the road into the velodrome.</li> </ul> <p><b>Talbot:</b></p> <ul style="list-style-type: none"> <li>• Ballarat Rd – need a better shoulder at the bridge</li> </ul>



Issue	Details
	<p>before Talbot.</p> <ul style="list-style-type: none"> <li>Maryborough, Talbot Rd, Bridge on Northside of Talbot Rd shoulders need improving. London Bridge needs widening or cycle bridge to side. Speed limit needs to be reduced.</li> </ul> <p>General</p> <ul style="list-style-type: none"> <li>Widen shoulders on roads to Clunes, Dunolly, Talbot and Avoca</li> <li>Shoulder on Maryborough/Timor Rd needs improving</li> <li>Parts of Majorca Rd require wider sealed shoulders.</li> <li>Install two "caution cyclists" signs on the two roads leaving Maryborough - Dunolly.</li> <li>Bendigo – Redesdale Rd needs upgrading [this is out of the Shire]</li> <li>Sides of the roads need to be improved.</li> </ul>
<p>New works required to improve cycling</p> <p>(21 responses)</p>	<p>Survey respondents believe that the following new developments should be undertaken to improve cycling on the road, on tracks or at cycling facilities:</p> <p>Bealiba:</p> <ul style="list-style-type: none"> <li>Need bike paths in and around Bealiba – 1</li> <li>Develop a loop trail from Main St, Bealiba to the Reservoir and back via Scent Farm Rd – 1</li> </ul> <p>Carisbrook:</p> <ul style="list-style-type: none"> <li>Need an off road trail from Cramer Rd / Williamson Crt to Carisbrook township along the Carisbrook Eddington Rd suitable for mobility scooter use – 2</li> </ul> <p>Dunolly:</p> <ul style="list-style-type: none"> <li>Construct a bike lane from Dunolly Primary School to Broadway – 1 [this lane already exists].</li> </ul> <p>Maryborough:</p> <ul style="list-style-type: none"> <li>Bristol Hill needs trails suitable for cycling and walking – 3.</li> <li>Cycle lane leading into Burns/ Tuaggra St roundabout from the railway underpass – 1.</li> <li>Need new trails or formalised trails all through the bush around Maryborough – 1.</li> <li>Track around MEC – 1.</li> <li>Remodel roundabout in Inkerman St / High St to improve safety for cyclists (cycle lane currently cuts out just prior to roundabout) – 1.</li> <li>Cycle lane in Tuaggra St, Inkerman St, Park Rd, Derby Rd – 1.</li> <li>Off road track at Mosquito Track and Cemetery Track should be a circuit track around town – 1.</li> </ul> <p>Talbot:</p>

Issue	Details
	<ul style="list-style-type: none"> <li>• Road between McCallums Creek Rd and Talbot (Majorca Rd) – 1.</li> </ul> <p>General:</p> <ul style="list-style-type: none"> <li>• Need more bike lanes – 1.</li> <li>• More bike racks around town – 1.</li> <li>• More sealed roads – 1.</li> <li>• Old Tullaroo Road – 1</li> <li>• Improve markings of trails through the bush and make them more diverse and challenging if possible) – 1.</li> <li>• Paint white line one bike width out from the edge of roads – 1.</li> <li>• Maintain shoulders on out of town roads so adequate space for riders to be off the road for oncoming/rear coming traffic – 1</li> <li>• An integrated bike pathway between Dunolly and Talbot through Maryborough – 1.</li> <li>• Bubble taps available at Talbot, Carisbrook, Maryborough, Bet Bet and Dunolly – 1.</li> </ul>
<p>Factors most important for ON road cycling</p> <p>(41 responses)</p>	<p>Survey respondents identified the following works as most important for on road cycling:</p> <ul style="list-style-type: none"> <li>• Wide shoulders on the road (90%)</li> <li>• Smooth sealed surface (90%)</li> <li>• Well maintained lanes (90%)</li> <li>• Clearly marked bike lanes (83%)</li> <li>• Regular signs advising motorists to watch out for cyclists (80%)</li> </ul>
<p>Factors most important for OFF road cycling</p> <p>(31 responses)</p>	<p>Survey respondents identified the following works as most important for off road cycling and mountain bike riding:</p> <ul style="list-style-type: none"> <li>• Tracks that provide an easy connection from homes to mountain bike tracks (68%)</li> <li>• Well maintained tracks (68%)</li> <li>• Downloadable maps (58%)</li> <li>• Tracks that are attractive and provide good views (58%)</li> <li>• Directional signs (52%)</li> <li>• Variety of loops for different skill levels (52%)</li> </ul>
<p>The most important thing for Council to do</p> <p>(31 responses)</p>	<p>Respondents were asked to identify the one thing that is most important for Council to do in relation to improving cycling opportunities in the Shire. They identified the following:</p> <ul style="list-style-type: none"> <li>• Construct more dedicated bike paths / lanes – 7</li> <li>• Construct wider shoulders on roads – 5</li> <li>• Encourage car and truck drivers to be more patient and acceptable of other road users. Also encourage bike riders to do the same - 4.</li> <li>• Have fun non-competitive community based / family cycling events – 2</li> <li>• Update and promote the Dunolly cycling routes with</li> </ul>

Issue	Details
	<p>signage, new maps etc. – 2</p> <ul style="list-style-type: none"> <li>• Bike lanes beside footpaths - 1</li> <li>• Increase bike parking at major facilities with security to encourage people to ride to more places for daily activities – 1</li> <li>• Promote the existing local map of rides better in the local community – 1</li> <li>• Provide maps – 1</li> <li>• Create a 3km loop track from Bealiba Main Street out to the local Reservoir along Scent Farm Road - 1.</li> <li>• Upgrade access to velodrome by sealing track/road – 1</li> <li>• Develop a track around MEC that could be used for Energy Breakthrough training, cycling and walking – 1</li> <li>• Develop a circuit track for Energy Breakthrough training - 1</li> <li>• Promote cycling as a form of transport – 1</li> <li>• Upgrade cycle tracks with signage to Maryborough's tourist attractions, e.g. Bull Gully Wells, Paddy Ranges, etc. – 1</li> <li>• Extend bike lanes to connect residents in outlying areas to their closest town – 1</li> <li>• Construct an off road trail along the Carisbrook Eddington Rd to the Cramer Rd / Carisbrook / Havelock Rd intersection - 1</li> <li>• Improve Bendigo – Redesdale Rd [outside Shire] – 1</li> </ul>
<p>Other comments (15 responses)</p>	<p>Respondents were asked to identify any other issues or opportunities in relation to cycling. They specifically mentioned the following :</p> <ul style="list-style-type: none"> <li>• Pleased with developments and facilities available for cycling – 3</li> <li>• Make sure that off road trails are suitable for mobility scooters – 2.</li> <li>• Additional paths/facilities for use of mobility scooters/walkers - 1</li> <li>• Better design of roundabouts for cyclists -1</li> <li>• Pave nature strips for walking/cycling /gopher driving - 1</li> <li>• Increase use of velodrome -1</li> <li>• Develop some single track MTB trails in the bush -1</li> <li>• Encourage the use of cycles, mobility scooters &amp; walking as an alternative to driving in the CBD -1.</li> <li>• More public transport to enable people to walk between options (i.e. bus) -1</li> <li>• Improve communication with cyclists about areas that are dangerous for cycling - 1</li> <li>• Improve bike access to the Shire's heritage and natural attractions for locals and tourists - 1.</li> </ul>

## 8.5 Walking and running survey

A survey was developed in order to seek information from the general public about walking and running issues and opportunities. Surveys were forwarded electronically to all of the organisations listed within Council's Community Directory. A total of 59 surveys were completed. 19% of surveys were completed by males and 81% by females. The majority of survey respondents are aged between 40-49 years and 60-69 years of age (24% each), followed by 30-39 year olds (17%), 50-59 year olds (12%), 15-19 year olds (7%) and 70-79 year olds (7%). The vast majority of surveys were completed by Maryborough residents (56%), followed by Bealiba (22%), Carisbrook (11%), Dunolly and Talbot (5% each).

Key issues and opportunities identified include:

Issue	Details
Walk/run frequency  (58 responses)	The majority of respondents walk/run: <ul style="list-style-type: none"> <li>• Daily (45%)</li> <li>• Several times per week (38%)</li> <li>• Once every few weeks (5%)</li> </ul>
Type of walker/runner  (59 responses)	Survey respondents describe themselves as a: <ul style="list-style-type: none"> <li>• Recreational walker – mostly on off road tracks/paths to key destinations such as parks/playgrounds, lakes/rivers, bush, sports facilities, other attractions, etc. These destinations may be local or may be in other places, e.g. when I'm on holiday (58%)</li> <li>• Utility walker – I walk as a form of transport, e.g. to the shops, rather than for recreational purposes (29%)</li> <li>• Fitness walker/runner – walk or run long distances regularly as part of my training (24%)</li> <li>• Other (24%) – included answers such as push pram around streets, dog walker, slow walker, at gym on the treadmill, fast walker, walk to visit friends, etc</li> <li>• Commuter walker –walk to work or school primarily on footpaths (20%)</li> <li>• Recreational jogger/runner – jog/run mostly on off road tracks/paths to or around key destinations such as parks/playgrounds, lakes/rivers, bush, sports facilities, etc. (20%)</li> </ul>
Where people walk/run  (57 responses)	The majority of people generally walk/run to: <ul style="list-style-type: none"> <li>• Lake/river (44%)</li> <li>• Bush (40%)</li> <li>• Shops (39%)</li> <li>• Park/playground (39%)</li> <li>• Anywhere – I don't plan where I'm going (32%)</li> <li>• Sports facilities (23%)</li> </ul>
Footpaths / tracks used (52 responses)	General <ul style="list-style-type: none"> <li>• Around town / residential areas – 7</li> <li>• Along local roads or verge of roads (sealed or gravel) – 6</li> <li>• Bush tracks – 3</li> <li>• Goldfields Track – 2</li> </ul>

Issue	Details
	<ul style="list-style-type: none"> <li>• Shops – 1</li> <li>• Footpaths – 1</li> <li>• Walking / cycling tracks – 1</li> <li>• Majorca Rd - 1</li> <li>• Lock – 1</li> <li>• Karsick - 1</li> </ul> <p>Bealiba</p> <ul style="list-style-type: none"> <li>• Main St – 5</li> <li>• Mount Rd – 2</li> <li>• Logan Rd – 2</li> <li>• Cochrane St - 2</li> <li>• Cochrane Creek tracks – 1</li> <li>• White Hills Rd – 1</li> <li>• Waltons Rd tracks through bushland west of Mount Rd – 1</li> <li>• Around Bealiba Reservoir - 1</li> <li>• Tracks leading to Mt Bealiba - 1</li> </ul> <p>Carisbrook</p> <ul style="list-style-type: none"> <li>• Maryborough to Carisbrook track - 3</li> </ul> <p>Dunolly</p> <ul style="list-style-type: none"> <li>• Broadway – 1</li> <li>• Maude St – 1</li> <li>• Thompson St – 1</li> <li>• Barkly St – 1</li> <li>• Market St - 1</li> </ul> <p>Maryborough</p> <ul style="list-style-type: none"> <li>• Lake Victoria – 18</li> <li>• Goldfields Reservoir – 12</li> <li>• Inkerman St - 3</li> <li>• Princes Park – 2</li> <li>• Gladstone St – 2</li> <li>• Park Rd – 2</li> <li>• High St – 2</li> <li>• Bush near the airport – 2</li> <li>• Railway St – 2</li> <li>• Lean St – 2</li> <li>• Burns St – 2</li> <li>• Woolworths / ALDI car park – 1</li> <li>• Near Shire depot – 1</li> <li>• Gillies St – 1</li> <li>• Tuaggra St – 1</li> <li>• Alma St – 1</li> <li>• Clarendon St – 1</li> <li>• Phelan Rd – 1</li> <li>• Nelson St - 1</li> </ul> <p>Talbot</p> <ul style="list-style-type: none"> <li>• Talbot golf course - 1</li> </ul>

Issue	Details
<p>Walk/run hours per week (58 responses)</p>	<p>In an average week, survey respondents were likely to walk/run:</p> <ul style="list-style-type: none"> <li>• 2-3 hours (34%)</li> <li>• 4-5 hours (19%)</li> <li>• 1 hour (10%)</li> <li>• 6-7 hours (9%)</li> <li>• 30 minutes (9%)</li> </ul>
<p>Average number of steps people take each day. (56 responses)</p>	<p>The majority of respondents identified that they:</p> <ul style="list-style-type: none"> <li>• Don't use a pedometer (73%)</li> <li>• 10,000 or more steps (16%)</li> <li>• 6,000-9,999 steps (4%)</li> <li>• 0-2,999 (2%)</li> <li>• 3,000-5,999 (2%)</li> <li>• It varies from day to day (2%)</li> <li>• Don't know (2%).</li> </ul>
<p>Enjoyable aspects of walking/running (57 responses)</p>	<p>Survey respondents identified the following factors that they most enjoy about walking / running:</p> <ul style="list-style-type: none"> <li>• I enjoy being outside in the fresh air (81%)</li> <li>• It helps me get fit/stay in shape (79%)</li> <li>• It's a good activity to do with friends/family (54%)</li> <li>• I get to know my neighbourhood and explore the local area (32%)</li> <li>• I like spending time with my dog when I walk (30%)</li> <li>• I know that it's good for the environment (non-polluting form of transport) (30%)</li> <li>• I enjoy the physical challenge (28%)</li> <li>• I enjoy exploring new areas by foot when I'm travelling (23%)</li> </ul>
<p>Events/programs that people are interested in (57 responses)</p>	<p>Survey respondents identified the following types of walking/running events/programs that are of most interest to them:</p> <ul style="list-style-type: none"> <li>• Fun Runs/ Walks or charity events (39%)</li> <li>• Bushwalks (33%)</li> <li>• Local community or charity walking/running events of 1-2 hours duration (33%)</li> <li>• None (23%)</li> <li>• Neighbourhood walking groups (21%)</li> <li>• Triathlons/ironman/marathons (16%)</li> <li>• Mass participation multi-day events, e.g. Oxfam Trailwalker Challenge (100km in 48 hours) (12%)</li> <li>• Running clubs, e.g. Hash House Harriers (7%)</li> </ul>
<p>Barriers to walking/ running or walking/running more often (48 responses)</p>	<p>Survey respondents identified the following factors as barriers that stop them from walking/running more often:</p> <ul style="list-style-type: none"> <li>• There are not enough paths/tracks nearby (35%)</li> <li>• The condition of the path/track surface (35%)</li> <li>• There are sections of my journey where there is no</li> </ul>

Issue	Details
	<ul style="list-style-type: none"> <li>path/track (33%)</li> <li>• I don't have time (27%)</li> <li>• I live too far away from where I want to go (23%)</li> <li>• Weather – too hot, too cold or too wet (23%)</li> </ul>
Enablers to walking / running (44 responses)	<ul style="list-style-type: none"> <li>• Nothing – I already walk / run regularly – 14</li> <li>• More dedicated walking / cycling paths - 6</li> <li>• More time – 5</li> <li>• Grade gravel roads – 2</li> <li>• Produce maps of tracks – 2</li> <li>• Better signage – 2</li> <li>• Improvements to existing paths – 2</li> <li>• Flat gutters to allow access to footpaths – 2</li> <li>• More motivation - 2</li> <li>• Improvement to my physical / mental health first before I walk – 2</li> <li>• A roadside track at a safe distance from vehicles – 1</li> <li>• A path from Carisbrook township to the residential settlement at Crameri Rd / Williamson Crt area – 1</li> <li>• More seats to enable me to rest – 1</li> <li>• Run programs to encourage walking and running – 1</li> <li>• Organise walks on a monthly basis – 1</li> <li>• Childcare – 1</li> <li>• Footpaths on both sides of the street - 1</li> <li>• Cooler weather – 1</li> <li>• Easier tracks from home – linkages – 1</li> <li>• Having a group of people to walk with – 1</li> <li>• Better maintenance of existing paths – 1</li> <li>• Enforce dogs on leashes – 1</li> <li>• Wider paths in areas that have high levels of use – 1</li> <li>• Lighting on tracks that are used at night – 1</li> <li>• Interactive information about walking – 1</li> <li>• Well designed pedestrian/ mobility aid corridors linking key retail outlets. (IGA to Woolworths, High St to Coles &amp; ALDI, Crameri's / ALDI to Coles) – 1</li> <li>• Designated running tracks – 1</li> <li>• Greater upkeep of back roads, e.g. Dunolly Cemetery - 1</li> </ul>
Walking / running club membership (10 responses)	<ul style="list-style-type: none"> <li>• Maryborough and District Triathlon Club – 2</li> <li>• Boroondara Bushwalking Club based in Kew (walk in CGS regularly) – 2</li> <li>• Ballarat Trail Blazers – 1</li> <li>• Informal walking group – 1</li> <li>• Carisbrook Brooks – 1</li> <li>• Bendigo Bushwalkers – 1</li> <li>• Essendon Bushwalking Club – 1</li> <li>• Maryborough Bushwalking Club – 1</li> </ul>
Reasons for no longer being part of a walking / running club (3 responses)	<ul style="list-style-type: none"> <li>• Moved away from the area – 2</li> <li>• Was stalked by a disturbed man during Thursday walking group – 1</li> </ul>
Existing footpaths / off road	General



Issue	Details
<p>tracks that need upgrading (32 responses)</p>	<ul style="list-style-type: none"> <li>• Level footpaths in towns – 1</li> <li>• Leave bush tracks in natural state – 1</li> <li>• Upgrade most gravel roads and side roads – 1</li> <li>• Just maintain trails - 1</li> </ul> <p>Bealiba</p> <ul style="list-style-type: none"> <li>• Upgrade Main St – 3</li> <li>• Place 80kmph signs on edge of Bealiba – 2</li> <li>• Upgrade paths from Bealiba Pony Club to Main Street - 2.</li> <li>• Cochrane Creek tracks - 1</li> <li>• Improve road surface up to Bealiba Reservoir (Water Basin) and Mount Bealiba – 1</li> <li>• Footpaths or roads along the town boundary of Logan Rd – 1</li> <li>• Resurface Scent Farm Rd or develop a suitable path alongside Scent Farm Rd out to the reservoir - 1.</li> </ul> <p>Carisbrook</p> <ul style="list-style-type: none"> <li>• Extend Maryborough to Carisbrook Track – 1</li> </ul> <p>Dunolly</p> <ul style="list-style-type: none"> <li>• On Broadway, continue footpath to both sides of the road further along in both directions – 1</li> <li>• Raglan St – 1</li> <li>• Hospital St – 1</li> <li>• Grade Peart Lane, Peart Road and Timbertop Road more often - 1.</li> </ul> <p>Maryborough</p> <ul style="list-style-type: none"> <li>• Seal Lake Victoria track – 2 [heritage listing prevents this from occurring]</li> <li>• The low wooden bridge at Goldfields Reservoir needs rails and something on board as it is slippery – 2</li> <li>• Extend the bike track near the velodrome to continue down Phelan’s Rd and down to the traffic lights at Gladstone St – 1</li> <li>• Better link MEC to walking and riding tracks – 1</li> <li>• Derby Road – 1</li> <li>• Goldfields Reservoir – 1</li> </ul> <p>Talbot</p> <ul style="list-style-type: none"> <li>• Nil</li> </ul>
<p>New footpaths / off road tracks that need to be developed (34 responses)</p>	<p>General</p> <ul style="list-style-type: none"> <li>• Permission to go through private land – 1</li> <li>• Adelaide Lead - a walking track or more sealed roads – 1</li> <li>• Tower area – 1</li> <li>• Finish the footpaths at the east end of Majorca Rd - 1</li> </ul> <p>Bealiba</p>

Issue	Details
	<ul style="list-style-type: none"> <li>• Develop all purpose walking tracks – 1</li> <li>• Link tracks over Tin Pot Hill through to Barkers Flat Rd then back to Bealiba via Turnstalls Rd – 1</li> <li>• Mount Rd - 1</li> <li>• After Main St and further up Cochrane St towards railway crossing - 1</li> <li>• Along the creek, behind town - 1</li> <li>• Coburn Lane – 1</li> <li>• Parts of Baxter Lane, Coburn Lane which winds around (now) to join with the Bealiba Logan Rd - 1</li> <li>• Road/area past Bealiba Pony Club (Park Lane) which continues past old silos and merges with Mount Rd - 1</li> <li>• Scent Farm Rd from town centre to the reservoir - 1</li> <li>• 1-2km circuit around Bealiba – 1</li> </ul> <p>Carisbrook</p> <ul style="list-style-type: none"> <li>• Between Carisbrook-Havelock Rd / Carisbrook-Eddington Rd / Crameri Rd intersection and Carisbrook township – 2</li> <li>• A path joining Bucknall Street to Smith Street in Carisbrook therefore creating a safe walking track from the local school to the football oval – 1</li> </ul> <p>Dunolly</p> <ul style="list-style-type: none"> <li>• Maude St needs footpaths to encourage parents to walk their children to preschool - 1</li> <li>• Connect the footpaths from Broadway to Maude St in several locations - 1</li> </ul> <p>Maryborough</p> <ul style="list-style-type: none"> <li>• Off road walking track around Bristol Hill - 2</li> <li>• Need a trail near the airport, going into the bush - 2</li> <li>• New footpath along Holyrood St - 1</li> <li>• Formalise tracks between Maryborough and Flagstaff - 1</li> <li>• Improve old track near velodrome - 1.</li> <li>• Palmerston St – 1</li> <li>• Raglan St – 1</li> <li>• Trail around MEC – on both the airport side and the Bristol Hill side – 1</li> <li>• Wider path around Lake Victoria – 1</li> <li>• Wider path around Goldfields Reservoir – 1</li> <li>• Develop a walkway across the centre of car park between (Napier/Alma/Nolan/Tuaggra Streets). This would help link IGA/Nolan St to Woolworths) – 1</li> <li>• McCallum’s Creek - 1</li> </ul> <p>Talbot</p> <ul style="list-style-type: none"> <li>• Nil</li> </ul>
Factors most important for walking and running paths/tracks	Respondents identified the following works as most important for walking and running paths/tracks:

Issue	Details
(58 responses)	<ul style="list-style-type: none"> <li>• Well maintained paths/tracks (79%)</li> <li>• Paths/tracks that provide an easy connection from homes to key destinations (69%)</li> <li>• Smooth, sealed surface (59%)</li> <li>• Good lighting along the path/track (59%)</li> <li>• Wide footpaths/tracks (53%)</li> <li>• Tracks that are attractive and provide good views (53%)</li> <li>• Directional signs (50%)</li> </ul>
The one thing that Council could do to improve walking / running opportunities (45 responses)	<ul style="list-style-type: none"> <li>• More lighting on trails, e.g. Lake Victoria and Goldfields Reservoir - 6</li> <li>• Just keep maintaining existing trails – 6</li> <li>• Produce maps of trails – 4</li> <li>• Organise walking challenges / charity fun events such as Mother’s Day Classic – 3</li> <li>• Increase awareness of trails and encourage people to use them – 3</li> <li>• Improve paths – 2</li> <li>• Improve gutters – 2</li> <li>• More seats – 2</li> <li>• More footpaths / walking tracks - 2</li> <li>• Organise walking groups / bushwalking groups – 2</li> <li>• Provide footpaths around preschools and schools – 2</li> <li>• Connect existing recreational areas by having paths linking Lake Victoria, Goldfields Reservoir and Bristol Hill - 2.</li> <li>• Install signs with historical information along trails - 2</li> <li>• Promote and provide at least 1 short walk (in local townships) and 1 longer option for example to local cemetery, Reservoir and better access up to Mt Bealiba - 1</li> <li>• Establish a monthly park run group - 1</li> <li>• Additional lighting along designated walking tracks or improved existing lighting, perhaps more solar lighting – 1</li> <li>• Grade roads – 1</li> <li>• Either seal Scent Farm Rd to Bealiba Reservoir or create a walking track to the Reservoir – 1</li> <li>• Ensure that tracks are smooth and wide enough for two to three people to walk together – 1</li> <li>• Stricter regulation of on dogs on lead rules e.g. around lake Victoria – 1</li> <li>• Roadside access into smaller towns for walkers and people using mobility scooters – 1</li> <li>• Safer crossings – 1</li> <li>• Construct a new path from Carisbrook township along the Carisbrook / Eddington Rd to service the households along Eddington Rd, Cramer Rd, Williamson Crt and Havelock Rd – 1</li> <li>• Install a trail in Bealiba – 1</li> <li>• Keep encouraging walkers who exercise their dogs to keep them on leads or confined to their property and to</li> </ul>

Issue	Details
	<p>pick up after them – 1</p> <ul style="list-style-type: none"> <li>• Seal some gravel paths - 1</li> </ul>
<p>Other comments (21 responses)</p>	<ul style="list-style-type: none"> <li>• Council does a good job with walking and cycling facilities – 4</li> <li>• Have a community walk four times per year – twice around Princes Park and Lake Victoria and twice around Goldfields Reservoir, with service clubs taking it in turn to provide a BBQ and drinks for sale.</li> <li>• Not aware of any organised programs - 2</li> <li>• Should have walking groups for people to join - 2</li> <li>• Would like a local running club to be established – 2</li> <li>• Need quality, safe paths to walk on to feel safe – 1</li> <li>• Facilities are adequate – just need to upgrade to a higher standard - 1</li> <li>• New tracks in the bush – 1</li> <li>• Trail from Massimo Crt primarily for school children – 1</li> <li>• Develop tracks around Lake Victoria – 1</li> <li>• Bristol Hill – could establish more formalised tracks – 1</li> <li>• Lake Victoria Track – is great to use - 1</li> <li>• Pedestrian Crossing in Tuaggra Street linking Coles and Crameris/ Aldi - 1</li> </ul>

## 8.6 Written submissions

Individual / organisation	Details
Jim Field	<p>Consider working in partnership with a service organisation such as Rotary to run a national education program about cyclists and motorists respecting each other on the roads.</p> <p>“London Bridge” is located on the Maryborough Ballarat Rd just about a km on the north side of Talbot. Cyclist’s use this road particularly, when there is a southerly wind blowing. The bridge is on a bend and is quite narrow and the road has very poor shoulders. It becomes extremely dangerous when cyclists are being overtaken at the same time that we have oncoming traffic passing. This scenario effectively makes three lanes of traffic on a two lane bridge.</p> <p>The preferable fix is to replace this bridge. If this is not possible then an alternative might be to put a foot/cycle bridge on the left side of the existing structure (going south). A third option may be increase signage to in order to prevent cyclist from being overtaken on the bridge including the immediate approaches.</p> <p>Both McCullum’s Creek Rd and Avoca Maryborough Rd also have very poor shoulders making it impossible for cyclists to safely move off the road when vehicles are passing.</p>
Bushwalking Victoria	<p>Bushwalking Victoria would like to see good quality mapping, good quality signage and better promotion of walking opportunities in the Shire. Ideally maps and signage would be completely compatible. Specific initiatives to be considered:</p> <ul style="list-style-type: none"> <li>• Work in partnership with Parks Victoria to promote walking opportunities in Paddy’s Ranges. Note that the existing Parks Victoria map does not name quite a few tracks on its Park Notes map and signage on the ground is poor.</li> <li>• Better publicise and sign the 14km Bealiba Circuit taking in Mt Bealiba (450m) and Bealiba Reservoir</li> <li>• Better publicise and sign the 14.5km loop walk from the Flynn memorial at Moliagul, past the Welcome Stranger memorial to the top of Mt Moliagul and return on another track.</li> <li>• Consider promoting walks just outside Shire boundaries, e.g. Ben Major (610m) and Mt Beckworth (635m).</li> <li>• Consider to develop or promote existing walks near waterways (as this is a key attraction for walkers), e.g. Cochranes Creek in Bealiba, Tullaroop and McCallums Creeks in Carisbrook, Burnt Creek and a series of ponds in Dunolly, Lake Victoria and Goldfields Reservoir in Maryborough and Back Creek in Talbot.</li> <li>• More prominently feature Aboriginal and goldfields heritage in walks.</li> <li>• Use interpretive signage to promote aspects of trails,</li> </ul>

Individual / organisation	Details
	<p>e.g. geological pasts, Aboriginal heritage, goldfields heritage, etc.</p> <ul style="list-style-type: none"> <li>• Develop or continue to promote trails where there is a hill or a good view: Mt Cameron (417m), Mt Mooloort (325m), Mt Hooghly (373m), Wiseman Hill (375m), Lane Hill, Fawcett Hill, Mt Greenoch, Peters Hill, Hard Hill, Daisy Hill and Mt Glasgow.</li> <li>• Explore the potential to acquire lakes, streams and focal points for future trails if economically viable to do so.</li> <li>• Consider developing agreements with landowners to allow public access to private land so that walkers can enjoy views from hills currently on private land.</li> <li>• Develop a promotional map of the Shire, showing boundaries, towns and features (similar to the one in Loddon Shire).</li> </ul>
Geoff Macilwain	Develop a mountain bike park potentially at Hamer Flora Reserve, based on the You Yangs model. [this is Parks Victoria managed land]
Sue Purchas	<p>Provide trails suitable for people using mobility scooters.</p> <p>Develop a walking / cycling track originating from Carisbrook township, travelling along the Eddington road and terminating at the start of Crameri Road. Residents along the Eddington Road, Newmarket Road, Crameri Road, Williamson Court and Havelock Road would all be able to access this track. This track should go along Eddington Road, cross the railway line at Carisbrook then take a right hand turn immediately after the railway line, pass the Carisbrook Railway station and lead into the Carisbrook Recreation Ground. From the recreation ground it is a short trip down Urquhart Street to the general store, the bakery, the pub and Market Reserve. At the Crameri Road termination point there should be a shelter, seating and water if possible. This junction is where parents come to collect their children from the school bus so shelter and seating here would benefit not just the users of the cycling / walking track.</p>

Location	Suggested works
	<ul style="list-style-type: none"> <li>• There are trails at Dowie Reserve that are managed by ageing volunteers. This arrangement may not be sustainable long-term.</li> <li>• Bindiis are a problem in Market Reserve in Carisbrook.</li> </ul>
<b>Dunolly</b>	<ul style="list-style-type: none"> <li>• Need to upgrade Dunolly trails so that they are suitable for hybrid bikes.</li> <li>• Dunolly trails need to be rated and some more diversity of trail experience offered.</li> <li>• Need more signs on the Dunolly trails, particularly for people wishing to travel in the opposite direction to existing signage.</li> <li>• Need to fix footpaths on side streets in Dunolly</li> <li>• Bindiis are a problem in Dunolly – from the hospital to the school in particular.</li> <li>• Develop a walking trail around the Dunolly Reservoir.</li> <li>• Install an interpretive sign outside Dunolly Bakery outlining the town heritage walk.</li> </ul>
<b>Eddington</b>	<ul style="list-style-type: none"> <li>• Develop a trail from Eddington to Hogan’s Bridge.</li> </ul>
<b>Maryborough</b>	<ul style="list-style-type: none"> <li>• Potential to establish a training facility at Bristol Hill and the velodrome for all types of cycling. Consider a skills course and BMX tracks.</li> <li>• Extension of Energy Breakthrough track in the adventure playground area of Princes Park (this has just been funded by State Government).</li> <li>• Derby Road towards Goldfields Reservoir – need to upgrade bitumen walking track that crosses driveways</li> <li>• Develop a safe crossing point over Pyrenees Highway near Bristol Hill</li> <li>• Explore development of paths along reserve areas (i.e. main drain) in Maryborough (away from vehicles) to provide access to work, schools, shops.</li> <li>• Consider installation of outdoor exercise equipment around Lake Victoria.</li> <li>• Park Road – Golf Course to Gardiner Crt in Maryborough needs a path.</li> <li>• Consider developing other recreational routes through the town, e.g. Goldfields Reservoir.</li> <li>• Consider reviewing gutters, touch pads and timing of traffic lights to make it easier for pedestrians.</li> <li>• Consider developing a number of formal mountain bike trails (perhaps in a circuit) in the bush around Maryborough with links into the town. Will need support from Parks Victoria.</li> <li>• Extend path on Gladstone St to Phelans Rd and back along Avoca Road</li> <li>• Need an off road area for Energy Breakthrough participants to train, e.g. Lake Victoria camping area, Hedges Oval, Carisbrook Recreation Reserve or Maryborough to Carisbrook Trail.</li> <li>• The corner of Balaclava Road and Inkerman St is considered a dangerous location by schools.</li> </ul>



Location	Suggested works
	<ul style="list-style-type: none"> <li>• Need a shared path from Gladstone St in the west around Phelan’s Road for school children to travel safely to MEC.</li> <li>• Highview College needs safe north, south, east and west routes. Need a path along Kars St (which could also link to MEC). There is some concern about passive surveillance in the Kars St bushland. Need a trail along Mariners Reef Road to provide a safe connection between Highview College and St Augustine’s School. Need a trail to cross from Derby St to Highview College (Churchill St / Mariners Reef Road).</li> <li>• A trail in Maryborough from Park St, left into Tullaroop, right into Burns St and on to Highview would be good.</li> <li>• Need a shared trail on Inkerman Street near the Early Learning Centre.</li> <li>• Need a footpath along Kars St from High Street.</li> <li>• Ensure that pedestrian crossings are linked to bus stops – particularly Napier St and Inkerman St.</li> <li>• Develop a circular cycling route around Maryborough, e.g. a trail that links Goldfields Reservoir, Maryborough Airport, Lake Victoria and Bristol Hill.</li> <li>• Young people would like dirt jumps to be reinstated in Maryborough, and would like to be involved in the development of the jumps.</li> <li>• Identify a principal pedestrian network for Maryborough – one route into town and one route to Maryborough Education Centre.</li> <li>• Need to have a marked, safe bike route (potentially painted) from Maryborough East to Maryborough Education Centre (MEC) to encourage students to ride to school.</li> <li>• Upgrade rail crossing on Derby Road (hopeful that Vic Track will cover the cost of this if it reinstates rail line to Ararat.</li> <li>• Need a pedestrian crossing with lights between Crameri and MKM – may need to relocate bus stop slightly.</li> <li>• At the Maryborough end of the Maryborough – Carisbrook Trail, trail users have to cross Tuaggra Street to get to the underpass. The trail stops at McDonalds. Cars park on both sides of Tuaggra Street. Would like to see the trail continue along Burns Street, onto Nolan Street and then finish at the Resource Centre in Alma Street where there is secure bike parking.</li> <li>• Seats on Paddy’s Ranges walking track need replacing.</li> <li>• Train service doesn’t work well for people wishing to access trails in the region via public transport, i.e. they need to stay one to two nights depending on their ride.</li> <li>• Need lights on main crossings in Maryborough.</li> <li>• Need a footpath in Holyrood Street.</li> <li>• Curb is too high on west side of High Street Maryborough for scooters.</li> <li>• Footpath needs to be level, e.g. Napier St going up to IGA.</li> <li>• Develop an off road track between Maryborough and Daisy Hill.</li> <li>• Like the flat gutters in High St, Maryborough – install more of these.</li> </ul>

Location	Suggested works
	<ul style="list-style-type: none"> <li>• Need a pedestrian crossing from the post office to the coffee shop.</li> <li>• Need footpaths to be constructed off Gladstone Street.</li> <li>• More dog excrement bags required throughout Maryborough parkland areas.</li> <li>• Develop a walking track around Maryborough Golf Course.</li> </ul>
<b>Moliagul</b>	<ul style="list-style-type: none"> <li>• Need to develop a trail around Moliagul.</li> </ul>
<b>Talbot</b>	<ul style="list-style-type: none"> <li>• Road shoulders need to be widened and roads graded, e.g. Talbot needs an apron (Ballarat – Maryborough Rd – London Bridge).</li> <li>• London Bridge needs an upgrade (cycle path on the side).</li> </ul>

## 10.0 Existing walking and cycling programs and events

The following walking and cycling programs occur in Central Goldfields Shire at present:

Organisation	Daisy Hill Community House	Dunolly & District Neighbourhood Centre	Goldfields Employment & Learning Centre Inc.	Maryborough Community House Inc.	Community Hub Station Domain	Healthy Together Grampians Goldfields	Dunolly Social Cyclists
<b>Walking group details</b>	No walking group	Walking group Monday morning 9:30am. \$0.50 fee to cover tea and a biscuit at the end. Approx. 6 people currently attending. Age group – 60+ years, one woman approx. 35 years Walk around the bushland in Dunolly	In 2015 coordinated a nature walk through the Paddy's Ranges. Single day event Small fee – approx. \$5 - \$10 Approx. 5 or 6 people attended Not sure if it will be offered in 2016 – will depend on interest. Unsure if it was offered/taken up in previous years	No walking group. Have attempted to coordinate groups previously. Lack of interest – 2 people attending. Tried different times, morning / evenings and different days of the week	Do not coordinate any groups. Only physical activity is gentle exercise held on a Friday morning. Participants are 70+ - 90 years – frail and have mobility problems.	This program no longer operates.	
<b>Cycling group details</b>	No cycling group	There is a group 'Dunolly Cyclists' who meet on a Sunday either fortnightly or monthly.				Does not currently host any cycling groups	Runs fortnightly. Organised social rides. The group isn't growing but is successful. Over summer months there

Organisation	Daisy Hill Community House	Dunolly & District Neighbourhood Centre	Goldfields Employment & Learning Centre Inc.	Maryborough Community House Inc.	Community Hub Station Domain	Healthy Together Grampians Goldfields	Dunolly Social Cyclists
							<p>are usually 8 – 10 participants, with 15+ over the cycling months. One ride in the month is a road ride and the second is on mountain bikes and hybrids. Always finish off with coffee at the end of the ride. The group has been operating for 18 months. The rides are social but also often coordinated to take in nature and historical sites.</p>

## Heart Foundation Volunteer Run Walking Groups

Name of Group	Details
<b>Princes Park Walkers</b>	Not operating as part of the Heart Foundation. Just a group of 4 or 5 that meet each morning at 8am and leave from the swimming pool
<b>Talbot Walkers</b>	No longer operational
<b>Maryborough CH Walkers</b>	No longer operating
<b>Dunolly Walkers</b>	Disbanded due to lack of interest

The following events currently take place in Central Goldfields Shire on a regular basis:

The major cycling event currently held in Central Goldfields Shire is the RACV Energy Breakthrough. This event is held annually and involves school students from all over the Australian eastern seaboard designing, constructing and then racing human powered vehicles. Over 400 teams comprising of thousands of students, teachers, parents and volunteers are involved in this four day event which is held each November in Maryborough. The event has been running for 25 years and is estimated to generate in excess of \$4 million to the local economy in recent years.

The World Mountain Bike Orienteering Championships were held in Central Goldfields Shire in 2004 and the Victorian State Series for Mountain Bike Orienteering (short and middle distance) event was held in 2013. Other events that have been held each year include the Central Goldfields Reverse Triathlon and the South Pacific Veteran's Cycling Championships (comprising of 45km and 60km road races, straight line 300m sprints, time trial and criteriums) and the 63km open handicap Cec Cripps Handicap. Periodically other events such as the Great Victorian

Bike Ride, Audax events or the Jayco Herald Sun Tour may also pass through the Shire.

The annual Relay for Life walking event attracts hundreds of participants and raises money for cancer research. The Spring into Summer Stroll is a fun day for the whole family focussing on walking and physical activity and Pyrenees Magic is a 113km charity bike ride organised by the Rotary Club of Maryborough with proceeds going to Maryborough and District Health Service.

In 2015 the Goldfields Employment & Learning Centre Inc. coordinated a nature walk through the Paddy's Ranges, although this was a one off event. Bushwalking clubs from Bendigo, Ballarat and Melbourne frequently bring their members to Central Goldfields Shire for a walk in the bush.

Potential Programs:

Type of program	Details
<b>Hiring a cycling contractor</b>	Hiring a contractor to provide various cycling related classes, programs or rides can encourage safe and proper riding behaviours in the community. Contractors can provide a variety of programs that serve all cycling disciplines, ages and riding abilities and also offer a progression of skills and techniques for various riding surfaces, amenities and elements, especially those that are available locally. The contractors should provide and be responsible for all management and supervision of program staff and any equipment necessary. Appointment could be made by tender.
<b>Learn to Bike Day Camp</b>	Aspiring cyclists aged 3-8 are nurtured by skilled, patient instructors in a supportive on-site environment to learn to ride in a half-day 'Learn to Bike' day camp. Participants are grouped by ability and work through a progression from scooting to coasting to pedalling as they get comfortable on two wheels. Learn to Bike is intended for first-time riders or kids using training wheels or balance bikes, not those who can already ride.
<b>Junior Mountain Bike Day Camp</b>	Cyclists aged 8-13 master mountain biking as they take part in a week-long Mountain Biking Day Camp. With help from experienced instructors, campers spend the week practicing mountain biking skills, learning bicycle maintenance basics and riding local trails. Children are grouped according to age and classes are customised so children program as their comfort and skill levels grow. Mountain Biking Day Camp campers should be comfortable riding on dirt on two wheels.
<b>Mountain Bike Skills Camp</b>	Over a period of a week, campers aged 8-13 years of age ride area trails, jumps and man-made features taking on increasingly challenging activities and exploring new terrain. Grouped by skill level and mentored by experienced instructors, campers learn the basics of urban and trail riding before mastering more advanced skills.
<b>Cycling Girls Camp</b>	Cycling Girls Camps give female riders from 11-14 years of age the opportunity to explore the fun and adventure of mountain biking in a supportive environment over a four day periods. Riders will increase confidence while guided by experienced and encouraging female coaches.
<b>"Keeping up with the Kids"</b>	Whether a parent or not, this half or full day program gives adult riders the skills they will need to keep up with their children
<b>Bike Mechanic Series</b>	A six-week course that gives students 16 years of age and over the skills necessary to work on and repair all basic systems of the bicycle in 2.5 hour sessions. Designed for beginner riders through to advanced riders.

Type of program	Details
<b>Beginner Rides</b>	These entry level rides for beginner riders give participants the chance to practice their skills, have fun, and be safe riding trails. Before each ride, volunteer Ride Guides review fundamental tips about riding, then the group heads out on an entry level trail or path to ride. There are two Ride Guides for this ride so nobody will get left behind. This is a family friendly, entry level ride.
<b>Mix and Mingle Rides</b>	These rides are for intermediate riders, taking place on different trails and paths year-round. Each ride is followed by dinner and drinks so with fellow riders.
<b>Trails and Ale Rides</b>	These intermediate rides are held mid-week. Riders travel at their own pace and regroup at intersections. The ride finishes at a pub.
<b>Girls Rides</b>	A weekly women-only ride includes the fun of riding with a group plus, post-ride get-togethers.
<b>Fix a Flat Clinic</b>	A free workshop to learn the easiest and fastest way to replace bike tyre tubes.
<b>School Cycling Clubs</b>	Teachers and volunteers teach, supervise and support young people of mixed-abilities and skills (including beginners) to ride along local trails and to learn how to fix bikes. The group uses personal and donated bikes, gear and helmets.
<b>Bike Shop, Club and Team Outreach</b>	Volunteers from local bike shops and bike clubs can offer introductory mountain biking adventures to at-risk youth that would otherwise not have this experience. Partnering with numerous youth agencies and schools, experienced cyclists can provide fun-filled, daylong mountain bike outings for small groups of children aged 10 and up.
<b>Earn-a-Bike Programs</b>	Using donated or salvaged bikes that need attention, repair and a good home, The Earn-a-Bike Program can provide disadvantaged youth (age 10-23) the opportunity to earn a bike of their own by completing a multi-session training course covering the fundamentals of bike repair/maintenance and the rules of safe cycling. Bike shops, schools and outreach programs can set this up with a few donated or used bikes and volunteer mechanics willing to share their skills.
<b>Youth Bicycle Ambassador</b>	A summer leadership development and employment opportunity for young people age 16-24. Youth engage in an earn-a-bike program, learn about the rules of the road, practice group riding, receive presentations from community leaders, and generate



Type of program	Details
Programs	their own solutions for improved transportation in their community. Recognizing the importance of youth voices in the bicycle advocacy, this program aims to elevate youth priorities, and provide a framework for increased youth participation in cycling.

Other potential programs identified through community consultation include:

- Individual walking / cycling challenges
- Community walking / cycling challenges
- School holiday cycling programs
- Social dog walking group
- Bicycle education programs
- Walking School Bus program
- Cycling School Bus program
- Social community bike rides
- Cycling buddy program
- Geo-caching
- Bicycle recycling program
- Park Run
- Bush walks
- Beginner walking programs
- Cycling festival
- Community running clubs

Many of these programs are dependent upon organisations such as bicycle shops, community based groups such as bushwalking or cycling clubs, schools, health agencies, neighbourhood houses and informal groups of local residents.

## 11.0 Design guidelines

### 11.1 Stakeholder feedback on design

Stakeholders were asked to provide some comments about trail design.

Key aspects raised by stakeholders include:

- The road reserve in Maryborough and Talbot is only 20 metres wide; hence there is not a lot of space to develop shared paths in these towns. Road reserves in Carisbrook and Dunolly, however, are 30 metres wide.
- Ideally trails will be three metres wide; hence some existing trails may need to be made wider. Most footpaths in the municipality are only 1.5m wide.
- Need to ensure that there is pedestrian access through car parks at shopping centres.
- Careful consideration needs to be given to plantings near walking / cycling paths so that fruit and leaves do not fall on the ground and become a hazard for path users.
- Need to ensure that trails / paths reflect the character of the town in which they are installed. Granitic sand is likely to better meet the heritage look of some towns.
- Granitic sand surface is the preferred surface for a recreational trail around town perimeters and is a cheaper surface than concrete or bitumen.
- Trail surfaces around schools and aged care facilities should be sealed.
- Potential conflicts are likely to arise if there are many trail users, if the path is too narrow, if people do not adhere to trail etiquette (e.g. keep to the left), if shared paths feature cyclists travelling at high speeds
- Bushwalkers prefer their own paths due to the potential speed of mountain bike riders in the bush, but if not possible, they prefer wider paths and a path off to the side in some spots where it's not possible to have a wider path.
- Sealing of shoulders on roads is important.

## 11.2 Provision of footpaths and cycle paths

Standard C21 of Clause 56.06-8 describes the provision of footpaths and cycle paths as set out by the Planning Provisions:

Type of road / street	Description	Footpath provision	Cycle path provision
<b>Access lane</b>	A side or rear lane principally providing access to parking on lots with another street frontage. 300 vehicles per day with traffic speed of 10kmph.	None. Carriageway designed as a shared zone and appropriately signed.	None
<b>Access place</b>	A minor street providing local residential access with shared traffic, pedestrian and recreation use, but with pedestrian priority. 300-1,000 vehicles per day with a traffic speed of 15kmph.	Not required if serving 5 dwellings or less and the carriageway is designed as a shared zone and appropriately signed.  <u>or</u>  1.5m wide footpath offset a minimum distance of 1m from the kerb.	None
<b>Access street – level 1</b>	A street providing local residential access where traffic is subservient, speed and volume are low and pedestrian and bicycle movements are facilitated. 1,000-2,000 vehicles per day with a traffic speed of 30kmph.	1.5m wide footpaths on both sides. Footpaths should be widened to 2.0m in vicinity of a school, shop or other activity centre. Be offset a minimum distance of 1m from the kerb.	Carriageway designed as a shared zone and appropriately signed.
<b>Access street – level 2</b>	A street providing local residential access where traffic is subservient, speed and volume are low and pedestrian and bicycle movements are facilitated. 2,000-3,000 vehicles per day with a traffic speed of 40kmph.	1.5m-wide footpaths on both sides. Footpaths should be widened to 2.0m in vicinity of a school, shop or other activity centre. Be offset a minimum distance of 1m from the kerb.	Carriageway designed as a shared zone and appropriately signed.

Type of road / street	Description	Footpath provision	Cycle path provision
<b>Connector street – level 1</b>	A street that carries higher volumes of traffic. It connects access places and access streets through and between neighbourhoods. 3,000 vehicles per day, with 50 kph reduced to 40 kph at schools and 20 kph at pedestrian and cycle crossing points.	<ul style="list-style-type: none"> <li>• 1.5m wide footpaths on both sides.</li> <li>• Footpath widened to a minimum 2.0m in the vicinity of a school, shop, public transport stop or other activity centre.</li> <li>• Footpaths offset a minimum distance of 1m from the kerb.</li> </ul>	<p>For on-street cycling, increase the minimum clear carriageway in each direction by:</p> <ul style="list-style-type: none"> <li>• 0.7m where the trafficable carriageway is shared by cyclists but no dedicated bicycle lane is marked on the carriageway; or</li> <li>• 1.5m where a trafficable carriageway is shared by cyclists but no dedicated bicycle lane is marked on the carriageway and there is a single lane in each direction separated by a raised trafficable median of at least 2.0m in width with mountable kerbs; or</li> <li>• 1.7m where a dedicated 1.7m wide bicycle lane is marked on the carriageway.</li> </ul>
<b>Connector street – level 2</b>	A street that carries higher volumes of traffic. It connects access places and access streets through and between neighbourhoods. 3,000-7,000 vehicles per day, with a traffic speed of 60 kph or 50km/h reduced to 40kph at schools.	<ul style="list-style-type: none"> <li>• 1.5m wide footpath on each side and 1.7m bicycle lanes on the carriageway; or</li> <li>• 2.5m wide shared foot and cycle path on both sides and no dedicated bicycle lanes marked on the carriageway.</li> <li>• Footpaths widened to a minimum of 2.0m in the vicinity of a school, shop, public transport stop or other activity centre.</li> <li>• Footpaths or shared foot and cycle</li> </ul>	<p>For on-street cycling, increase the minimum clear carriageway in each direction by:</p> <ul style="list-style-type: none"> <li>• 0.7m where the trafficable carriageway is shared by cyclists but no dedicated bicycle lane is marked on the carriageway; or</li> <li>• 1.7m where a dedicated 1.7m wide dedicated bicycle lane is marked on the carriageway</li> <li>• 0.3m where there are two trafficable</li> </ul>

Type of road / street	Description	Footpath provision	Cycle path provision
		paths offset a minimum distance of 1m from the kerb.	lanes in each direction separated by a non-trafficable central median and the carriageways are shared by cyclists but no dedicated bicycle lane is marked on the carriageway; or <ul style="list-style-type: none"> <li>0.5m where there are two trafficable lanes in each direction separated by a non-trafficable central median and a 1.7m wide dedicated bicycle lane is marked on the carriageway.</li> </ul>
<b>Arterial road</b>	Greater than 7,000 vehicles per day. Speed is dictated as per relevant roads authority.	3m wide shared path on each side or as otherwise required by the relevant roads authority.	3m wide shared path on each side or as otherwise required by the relevant roads authority.

### 11.3 Path width

Some specific details are provided below in relation to path width<sup>10</sup>:

Type of path	Desirable minimum width (m)	Minimum width – typical maximum (m)
Bicycle path – local access path	2.5	2.5-3
Bicycle path – major path	3.0	2.5-4
Shared path – local access path	2.5	2.5-3
Shared path - commuter	3.0	2.5-4
Shared path – recreational	3.5	3-4
Separated two way path – bicycle path	2.5	2-3
Separated two way path – footpath	2.0	Greater than or equal to 1.5
Separated two way path - total	4.5	Greater than or equal to 4.5

<sup>10</sup> Austroads (2014) Cycling Aspects of Austroads Guides

Type of path	Desirable minimum width (m)	Minimum width – typical maximum (m)
Separated one way path – bicycle path	1.5	1.2-2
Separated one way path – footpath	1.5	Greater than or equal to 1.2
Separated one way path – total	3	Greater than or equal to 3.4

## 11.4 Shared paths

Bicycle Network<sup>11</sup> has identified the following criteria to encourage good design of shared paths:

- “Adequate width. Paths need to allow space for pedestrians to walk two abreast and leave room for on-coming traffic or bike riders to overtake.
- Gentle alignment, gradients and slopes. Steep grades can create a hazard for cyclists travelling downhill and difficulty when riding up. Sharp turns should be avoided as they are detrimental to the riding experience and are a potential conflict point when people cut the corner.
- Fences and edge of path treatments and edge of path clearances. Recommended minimum clearance from fixed objects alongside the path such as trees, rocks or poles is 1.0m. This allows space for path users to move off the path when stopped and provides a verge for dog walking.
- Appropriate path materials including for boardwalks and timber surfaces. Paths with a good quality surface will attract more users than a poorly surfaced path.
- Shared paths should be aligned to intersect roads at approximately 90 degrees to maximise sight distances. See road crossings, kerb ramps, middle of path clearances and underpasses
- Appropriate lighting as well as signage & navigation.”

<sup>11</sup> Bicycle Network: <https://www.bicyclenetwork.com.au/general/for-government-and-business/2854/> (viewed 8 Feb 2016)

## 11.5 Off-road walking tracks

### Land Manager Technical Decision Matrix for Grading Walking Tracks.

Grade of walk	Technical description				
	Grade 1	Grade 2	Grade 3	Grade 4	Grade 5
<b>Symbol</b>					
<b>Distance</b>	Total distance of track must not exceed 5km.	Total distance of track must not exceed 10km.	Total distance of track must not exceed 20km.	Total distance of track may be greater than 20km. Distance does not influence grading.	Total distance of track may be greater than 20km. Distance does not influence grading.
<b>Gradient</b>	Grades in accordance with the AS 1428 series. (AS 2165.1). A ramp at 1:14 (7.14% slope or 4.1 degrees) is the maximum slope/gradient suitable for a person in a wheelchair.	The gradient is generally no steeper than 1:10 (or 10% or 5.7 degrees). (AS 2165.1).	May exceed 1:10 (or 10% or 5.7 degrees) for short sections but generally no steeper than 1:10. (AS 2165.1).	May have arduous climbs and steep sections. May include long steep sections exceeding 1:10 (or 10% or 5.7 degrees).	May have very arduous climbs and steep sections. May include long steep sections exceeding 1:10 (or 10% or 5.7 degrees).
<b>Quality of path</b>	Broad, hard surfaced track suitable for wheelchair use. Width: 1200mm or more. Well maintained with minimal intrusions. (AS 2165.1).	Generally a modified or hardened surface. Width: 900mm or more. Well maintained with minimal intrusions. (AS 2165.1).	Formed earthen track, few obstacles. Generally a modified surface, sections may be hardened. Width: variable and less than 1200mm. Kept mostly clear of intrusions and obstacles. (AS 2165.1).	Generally distinct without major modification to the ground. Encounters with fallen debris and other obstacles are likely. (AS 2165.1). Walkers may encounter natural obstacles (eg tides).	No modification of the natural environment. (AS 2165.1).
<b>Quality of markings</b>	Track head signage & route markers at intersections.	Track head signage & route markers at intersections.	Track head signage & route markers at intersections and where track is indistinct.	Track head signage & route markers.	Signage is generally not provided. (AS 2165.1)
<b>Experience</b>	Users need no previous	Users need no previous	Users need no	Users require a moderate	Users require previous








Grade of walk	Technical description				
	Grade 1	Grade 2	Grade 3	Grade 4	Grade 5
<b>required</b>	experience and are expected to exercise normal care regarding their personal safety. (AS 2165.1)	experience and are expected to exercise normal care regarding their personal safety. (AS 2165.1). Suitable for most ages and fitness levels.	bushwalking experience and a minimum level of specialised skills. Users may encounter natural hazards such as steep slopes, unstable surfaces and minor water crossings. They are responsible for their own safety. (AS 2165.1).	level of specialised skills such as navigation skills. Users may require maps and navigation equipment to successfully complete the track. Users need to be self-reliant particularly in regard to emergency first aid and possible weather hazards. (AS 2165.1).	experience in the outdoors and a high level of specialised skills such as navigation skills. Users will generally require a map and navigation equipment to complete the track. Users need to be self-reliant, particularly in regard to emergency first aid and possible weather hazards. (AS 2165.1).
<b>Time</b>	30 minute increments (e.g. 1-1.5hrs) or if the predicted time is less than an hour in 15 minute increments (e.g. 30-45 minutes).	30 minute increments (e.g. 1.5-2hrs) or if the predicted time is less than an hour in 15 minute increments (e.g. 30-45 minutes).	Hour/days (e.g. 9hrs) or if the predicted time is less than an hour to the nearest 15 minute interval (e.g. 45 minutes)	Not applicable.	Not applicable.
<b>Steps</b>	Steps allowed only with alternate ramp access. (AS 2165.1).	Minimal use of steps. (AS 2165.1).	Steps may be common. (AS 2165.1).	N/A (AS 2165.1). Steps do not influence grading.	N/A (AS 2165.1). Steps do not influence grading.

## 11.6 Mountain bike tracks

IMBA Australia has produced a mountain bike trail difficulty rating system<sup>12</sup>:

### IMBA AUSTRALIA MOUNTAIN BIKE TRAIL DIFFICULTY RATING SYSTEM

	Very easy  White Circle	Easy  Green Circle	Intermediate  Blue Square	Difficult  Single Black Diamond	Extreme  Double Black Diamond
<b>Description</b>	Likely to be a fire road or wide single track with a gentle gradient, smooth surface and free of obstacles.  Frequent encounters are likely with other cyclists, walkers, runners and horse riders.	Likely to be a combination of fire road or wide single track with a gentle gradient, smooth surface and relatively free of unavoidable obstacles.  Short sections may exceed these criteria.  Frequent encounters are likely with walkers, runners, horse riders and other cyclists.	Likely to be a single trail with moderate gradients, variable surface and obstacles.	Likely to be a challenging single trail with steep gradients, variable surface and many obstacles.	Extremely difficult trails will incorporate very steep gradients, highly variable surface and unavoidable, severe obstacles.
<b>Suitable for</b>	Beginner/ novice cyclists. Basic bike skills required. Suitable for most bikes.	Beginner/ novice mountain bikers. Basic mountain bike skills required. Suitable for off-road bikes.	Skilled mountain bikers. Suitable for mountain bikes.	Experienced mountain bikers with good skills. Suitable for better quality mountain bikes.	Highly experienced mountain bikers with excellent skills. Suitable for quality mountain bikes.
<b>Fitness Level</b>	Most people in good health.	Most people in good health.	A good standard of fitness.	Higher level of fitness.	Higher level of fitness.
<b>Trail Width</b>	Two riders can ride side by side.	Shoulder width or greater.	Handlebar width or greater.	Can be less than handlebar width.	Can be less than handlebar width.
<b>Trail Surface and obstacles</b>	Hardened with no challenging features on the trail.	Mostly firm and stable. Trail may have obstacles such as logs, roots and rocks.	Possible sections of rocky or loose tread. Trail will have obstacles such as logs, roots and rocks.	Variable and challenging. Unavoidable obstacles such as logs, roots, rocks drop-offs or constructed obstacles.	Widely variable and unpredictable. Expect large, committing and unavoidable obstacles.
<b>Trail Gradient</b>	Climbs and descents are mostly shallow.	Climbs and descents are mostly shallow., but trail may include some moderately steep sections.	Mostly moderate gradients but may include steep sections.	Contains steeper descents or climbs.	Expect prolonged steep, loose and rocky descents or climbs.

<sup>12</sup> <https://parks.dpaw.wa.gov.au/sites/default/files/docs/activities/IMBA%20AUSTRALIA%20MOUNTAIN%20BIKE%20TRAIL%20DIFFICULTY%20RATING%20SYSTEM.pdf>

## 12.0 Urban bike park concept

One of the suggestions that was identified through consultation is the desire to further develop Bristol Hill as a bike hub or an urban bike park. Key components of this hub or park could include greater use of the velodrome; usage of the existing club house for bicycle education and club based activities; development of additional cycling tracks; and potentially features that are typically found in bike parks.

An urban bike park is essentially a purpose-built cycling facility for recreation and progressive skills development. Urban bike parks in the US and internationally typically feature dirt jumps; ramps; sealed learn to ride tracks for toddlers; family trails; a variety of natural 'rideable' objects such as logs, stairs, sand and rocks; infrastructure for events; and amenities.

Whilst it is acknowledged that this concept needs further exploration, the following section provides an overview of potential bike park elements, size requirements and costings.

It must be noted that the list of bike-specific features and trail types described below is not exhaustive. The cost estimates (given in Australian dollars, but based on US costs) are only rough guidelines. Every venue presents variables that will impact the final price. The estimated cost reflects the breakdown for the individual expenses for design, materials, labour, equipment, and the contingency for unforeseen challenges. The estimates do not include additional expenses related to site grading, sub-surface drainage needs, water supply, or landscaping costs.

In order to achieve the best outcomes, it is advisable to hire a professional bike park consultant to help guide the design-build process.

The ideal design process is “iterative,” meaning that it happens over time, with multiple opportunities for input and revisions. It is an ever-tightening loop that gives consideration to diverse factors, including rider preferences, available funds for the build as well as available resources for ongoing maintenance. Smaller-scale projects might require just a few rounds of design discussion, but larger parks may require multiple design iterations to satisfy the goals of stakeholders while meeting the constraints put forth by the park land manager. Bike parks can also be built in stages.

The City of Greater Geelong has recently installed a mountain bike park in an urban area of Geelong, which includes some of the elements that would be proposed in an urban bike park in Maryborough. The facility in Geelong features 3km of cross country trails, a skills development area, a pump park, dirt jumps, car parking and landscaping spread across a four hectare site. The aim of the park is provide a space for riders to hone their skills before progressing to more intermediate facilities. Consequently both blue and green rated features which are suitable for beginner to intermediate riders have been constructed.

Feedback from the City of Greater Geelong indicates that the site experiences a very high amount of use by all ages and abilities and is viewed positively by the community. Council's existing insurance policy has been deemed adequate to cover any potential risks at the site. The cost of the project was approximately \$250,000.



Figure 1: Marnock Vale Mountain Bike Park, Geelong



Figure 2: Timber structure at Marnock Vale Mountain Bike Park, Geelong



## 12.1 Typical features of urban bike parks

**Pumptracks** frequently provide the core of a bike park facility. Constructed on flat, open ground, they appeal to children and adults alike and provide a great location for learning and improving mountain bike skills. The key features of a pumptrack are the rollers and berms of varying height and frequency, depending on the intended users. These undulations allow a skilled rider to navigate the track without pedalling.

- Typical size: 0.25 to 2.0 acres (0.1 to 0.8 hectares)
- Average construction cost: \$14,000 to \$40,000



**Dirt jumps** allow bicyclists to take flight by launching themselves from earthen mounds or constructed features. Built on flat or slightly inclined slope, dirt jumps typically have several parallel alignments to accommodate beginners through expert riders. Building good jumps is as much art as science, with a considerable amount of effort put into shaping and maintaining the perfect “lip” or take-off point of soil features.

- Typical size: 0.5 to 3.0 acres (0.2 to 1.2 hectares)
- Average construction cost: \$14,000 to \$80,000.



**Flow trails** offer a roller coaster type of dirt trail arranged linearly, like a traditional cycling or hiking trail, that appeals to both new and seasoned riders. Features such as berms, dips, and accelerators turn up the fun factor and keep riders coming back for more.

- Typical size: 2.0 to 5.0 acres (0.8 to 2.0 hectares)
- Average construction cost: \$14,000 to \$95,000.



**Slopestyle courses** are similar to flow trails in that they are gravity-fed, but these trails include features such as wall rides and box jumps to vary the technical challenge. Costs will increase if the facility is intended to be used for sanctioned competition events.

- Typical size: 2.0 to 5.0 acres (0.8 to 2.0 hectares)
- Average construction cost: \$40,000 to \$200,000.





**Skills areas** are spiced with features such as ladder bridges, rock gardens, and log features, allowing beginner riders to acclimate to features typically found on traditional singletrack trails. Progressive skills areas also challenge more experienced riders with drop and/or jump zones.

- Typical size: 0.5 to 2.0 acres (0.2 to 0.8 hectares)
- Average construction cost: \$7,000 to \$35,000.



**Novice tracks** are intended for those who are just getting into mountain biking. These areas often feature small rollers and berms to encourage experimentation and build confidence. Rather than dirt, these areas may be constructed with an improved surface such as painted concrete, asphalt, or a rubberized playground surface. Locating this track adjacent to a traditional playground structure can help provide a centralized activity area and further strengthen the inclusive recreational aspect of a bike park.

- Typical size: 0.25 acres or less (0.1 hectare or less)
- Average construction cost: \$14,000 to \$40,000.





**Singletrack trails** resemble the traditional mountain bike venue of multi-use, natural surface trails that most mountain bikers are familiar with riding. They can provide a varied experience for bike park visitors and should be included if sufficient land is available—often designed around the perimeter of the site. The singletrack can be integrated into and shared with the other trail facilities to achieve cost efficiencies.

- Typical size: 5.0 or more acres (2.0 or more hectares)
- Average construction cost: \$30,000 to \$80,000.



**Dual slalom courses** provide a popular format for head-to-head competition in a compact, spectator-friendly environment. Requiring at least a roll-in ramp, and preferably situated on a gently sloping hillside, these tracks consist of parallel, duplicate tracks peppered with berms, rollers, and jumps. Although used for an established competition discipline, they are not just for racing and are popular with non-competitive riders. Costs will increase if the facility is to be used for sanctioned races.

- Typical size: 1.0 to 2.0 acres (0.4 to 0.8 hectares)
- Average construction cost: \$30,000 to \$70,000.



**Mountain bike or short track race courses** combine different types of trails with wide paths and roads used for maintenance. It may be possible to create competition-friendly race courses for various types of cycling. Costs will increase if the facility is to be used for sanctioned races vs. Local club races.

- Typical size: 2.0 acres (0.8 hectares) NB: the total distance will be longer because multiple trails are incorporated)
- Average construction cost: \$30,000 to \$300,000.

**Pre-engineered features** are popular trends in bike park design using features created with materials like steel, wood, and concrete rather than sculpting jumps, turns, and other riding elements with dirt and rock. In many cases, they are fabricated wholly or partially off-site. The advantages include durability, reduced maintenance, and consistency. Dirt features can degrade quickly, especially in the absence of proper maintenance, or they may shift away from the intended riding experience because of the need for regular maintenance. Riders, planners, and other partners involved in managing the bike park often find the consistency and engineering specifications of pre-engineered features reassuring.

- Typical size: ranges greatly, from pumptracks and small ladder bridges to ramps, wall rides, and jumps 20 or more feet (6 metres or more) in height.
- Average construction cost: \$1,300 to \$130,000 or more.



**Community-specific elements** are the defining characteristic of modern bike parks that incorporate many of the amenities and features of traditional parks. By adding these features, modern bike parks become strong assets for the communities that support them. At first, mountain bikers might see the construction of a gazebo, or the planting of ground cover or shrubs, as unneeded expenses. But these elements can be vitally important for building support and accommodating all ages and abilities in the community in the park to participate, learn or watch. A partial list of traditional park elements that can be successfully integrated into bike parks includes:

- Benches, picnic tables and other places to sit
- Grassy areas and landscaped terrain
- Parking lots (large enough to handle events)
- Playgrounds and play elements for non-riders
- Toilets and water fountains
- Shade structures.



## 13.0 Previous strategy achievements and required infrastructure works

The previous Cycling Strategy, which was completed in 2003, incorporated a suite of trail infrastructure projects along with recommendations for additional signage, education programs, community bicycle programs, supporting infrastructure (e.g. bike racks), promotions, commercial opportunities, improving accessibility, improving management of trails and introducing a more effective maintenance program. A significant number of trail projects were completed during the 10 years of the plan, but other areas such as signage and programs are still requiring a great focus.

Another key achievement of the previous strategy was the formation of a Bicycle Advisory Group to provide advice to Central Goldfields Shire on cycling initiatives. This group has worked effectively and achieved some good results.

The following table identifies the infrastructure projects which were included in the last strategy and provides commentary on which actions were achieved and which ones were not. It also identifies any works that may be required, with indicative costings. In addition, it lists the location, type and length of existing paths.

**Legend**

<b>Completed Works</b>
<b>Partially Completed</b>
<b>Not Completed</b>

**Legend**

<b>Existing Bicycle Paths April 2016</b>
<b>Proposed Works</b>
<b>Completed Works to April 2016</b>
<b>Required Works</b>
<b>Required Pedestrian Paths</b>

Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016		
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)
<b>Maryborough</b>															
M1	Maryborough Golf Course Track - extend to Ross	Extend off road path Sealed Bitumen	400	Medium	2010	18,000		Park Rd in front of Whirrakee	Concrete Shared Path	220	2008		Park Rd in front of Whirrakee	Concrete Shared Path	220
								Maryborough Golf Course Frontage	Asphalt Shared Path	325	2010		Park Rd Maryborough Golf Course Frontage	Asphalt Shared Path	325
								Maryborough Golf Course Club house and car park Frontage	Shared Parking Pedestrian Zone	215	2015		Maryborough Golf Course Club house and car park Frontage	Asphalt Shared Path	215
							<b>Mb2</b>	End of Golf Course Path to Gardiner Court	Concrete Shared Path	440	High	100,000			
M2	Pekin Road bw Gladstone St & Elliot St	Construct new on-road path	600	High	2005	2,200		Pekin Rd Gladstone St to Waters St	Concrete Shared Path	170	2006		Pekin Rd Gladstone St to Waters St	Concrete Shared Path	170
							<b>Mb15</b>	Pekin Rd Gladstone St to Elliot St	On Road Path Symbols & Signage Required	575	2014	2,000	Pekin Rd Gladstone St to Elliot St	On Road Path	575



Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016		
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)
M3	Gladstone St bw Mb Golf Course along Balaclava Rd and up to Precinct Drive	Part Gravel off road and part on road and upgrade existing concrete path	2500	High	2004	42,600		Gladstone St 325 to 287	Concrete Shared Path	390	2010		Gladstone St 325 to 287	Concrete Shared Path	390
								Gladstone St 287 to Dundas Rd	Asphalt Shared Path	420	2006		Gladstone St 287 to Dundas Rd	Asphalt Shared Path	420
								Gladstone St Dundas Rd to Balaclava Rd	Existing Concrete Shared Path widened	950	2006		Gladstone St Dundas Rd to Balaclava Rd	Concrete Shared Path	950
								Balaclava Rd Gladstone St to Precinct Dv	Concrete Shared Path	630	2006		Balaclava Rd Gladstone St to Precinct Dv	Concrete Shared Path	630
								Gladstone St Field St to Pekin Rd Balaclava Rd	On Road	1,240	2006		Gladstone St Field St to Pekin Rd Balaclava Rd	On Road Path	1,240
M4	Precinct Drive bw Gladstone St & Balaclava Rd	Off Road Gravel Path	450	Medium	2007	16,000	Mb16	Precinct Drive bw Gladstone St & Balaclava Rd	Off Road Gravel Path	240	Medium	20,000			

Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016		
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)
M5	Waters St bw Pekin Rd to rear of Education Centre	Off Road Gravel Path	550	High	2008	11,500	Mb21	Waters St bw Pekin Rd to rear of Education Centre (Hibberd St)	Off Road Gravel Path	170	Low	17,000			
M6	Brassey St, Laidman & Hughes St bw Pekin Rd & Dundas Rd	On Road	1150	Medium	2007	4,400		Not Required							
M7	Lake Rd	Extend Asphalt off road around Lake Victoria	1100	High	2006	59,000		Lake Rd	Gravel off road around Lake Victoria. Gravel preferred because of Heritage Precinct	1100	2007		Lake Rd around Lake Victoria	Gravel Shared Path	1100
													Princes Park	Concrete Shared Path	300
													Princes Park	Asphalt Shared Path	620
M8	Goldsmith St bw Inkerman St & Kars St	On Road Path	250	High	2006	1,250	Mb22	Goldsmith St bw Inkerman St & Kars St	On Road Path	250	Low	1,000			

Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016		
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)
M9	Kars St bw Goldsmith St & Gladstone St	Gravel Off Road Shared Path	300	High	2005	11,200		Kars St bw Goldsmith St & Gladstone St	Gravel Off Road Shared Path	340	2006		Kars St bw Goldsmith St & Gladstone St	Gravel Shared Path	340
								Shared Path bw Gladstone St & Balaclava Rd	Asphalt shared Path	100	2006		Shared Path bw Gladstone St & Balaclava Rd	Asphalt shared Path	100
M10	Newton St & Nightingale St bw Park Rd & Gladstone St	On Road Path	960	Medium	2007	4,000	Mb23	Newton St & Nightingale St bw Park Rd & Gladstone St	On Road Path	940	Low	4,000			
M11	Dodds St From Elgin Rd	On Road Path	250	Low	2012	1,200	Mb24	Dodds St From Elgin Rd To Crimera St	On Road Path	250	Low	2,000			
M12	Holyrood St bw Burns St & Park Rd	On Road Path	400	Medium	2008	1,500	Mb17	Holyrood St bw Burns St & Park Rd	On Road Path & Bridge Widening over Main Drain	400	Medium	40,000			
M13	Kars St bw High St & Burns St	On Road Path	400	Medium	2007	1,500	Mb18	Kars St bw High St & Burns St	On Road Path	400	Medium	2,000			
M14	Lean St from Railway St to Derby Rd	On Road Path	1,500	Medium	2009	5,000	Mb19	Lean St from Railway St to Derby Rd	On Road Path Symbols & Signage Required	470	2013	2,000	Lean St from Railway St to Derby Rd	On Road Path Symbols & Signage Required	470



Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016		
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)
							Mb3	Railway St Lean St to Gillies St	On Road Path/ Some Shoulder sealing Symbols & Signage Required	600	High	15,000			
							Mb4	Gillies St Railway St to Majorca Rd	On Road Path/ Some Shoulder sealing Symbols & Signage Required	400	High	7,000			
								Gillies St Majorca Rd to Sutton Rd	On Road Path/	1,000	2015		Gillies St Majorca Rd to Sutton Rd	On Road Path/	1,000
							Mb28	Pekin Rd Elliot St to Fink St	On Road Path Symbols & Signage Required	200	Medium	2,000			
							Mb5	Gillies St Sutton Rd to Mc Lure St	On Road Path/ Symbols & Signage Required	770	High	4,000			
							Mb6	Mc Lure St Gillies St to Tullaroop Rd	On Road Path/ Some Shoulder sealing Symbols & Signage Required	890	High	7,000			

Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016		
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)
							Mb25	Tullaroop Rd McLure St to Park Road Maryborough	On Road Path	585	Low	4,000			
M15	Argyle Rd bw Gillies St & Cemetery Wright St	On Road Path	1,300	Low	2012	4,100	Mb26	Argyle Rd bw Gillies St & Cemetery Wright St	On Road Path Not Wide enough, should be Off Road Gravel)	1,300	Low	130,000			
M16	Giddings St & Taylor St to half way bw Kennedy St & Primrose St	On Road Path	600	Medium	2008	3,000	Mb20	Giddings St & Taylor St to half way bw Kennedy St & Primrose St	Should be Off Road Concrete Path	600	Medium	120,000			
M17	Albert St & Station St bw Inkerman St & Tuaggra St	On Road Path	450	High	2006	2,500	Mb7	Albert St, Station St and Burns St bw Inkerman St & Tuaggra St	Should be Off Road Concrete Path	450	High, as it Links with Railway Station	90,000			
M18	Fink St bw Pekin Rd to Griffith St & to Pekin Rd to Elliot St	Gravel Off Road Shared Path & part On Road Path	2,080-1,300 gravel, 780 on road	Low	2013	30,500	Mb27	Fink St bw Pekin Rd to Griffith St & to Pekin Rd to Elliot St	Gravel Off Road Shared Path & part On Road Path 2,080-1,300 gravel, 780 on road	2,080	Low	40,000			

Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016		
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)
M19	Bristol Hill Reserve	Upgrade Existing Gravel Tracks	2,000	Medium	2008	20,000	Mb51	Bristol Hill Reserve	Upgrade Existing Gravel Tracks	2,000	Medium	25,000	Bristol Hill Reserve	Gravel Tracks	2,000
							Mb8	Veledrome Entrance off Elgin Rd	Seal Roadway and Seal Car Park	150	High	50,000			
							Mb9	High St from Inkerman St to Lamb St	On Road Path Very Narrow, consider off road widened footpath?	825	High	165,000			
							Mb10	Along Elgin Rd from Lamb St, along Phelans Rd, then along Gladstone St to Precinct Dr	Off Road Gravel Shared Path	1,400	High, as would generate much use	140,000	Along Elgin Rd from Lamb St, to Phelans Rd	Off Road Gravel Shared Path	700
							Mb37	Elgin Rd from Lamb St to Veledrome	Off Road Gravel Shared Path	250	Medium	25,000			

Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016		
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)
							Mb11	Derby Rd from Rinaldi Dr to Reservoir Rd	Upgrade Existing Bitumen Shared Path to Concrete Path (including Derby Rd Rail Bicycle Pedestrian Crossing)	875	High, as a very highly used area	180,000	Derby Rd from Rinaldi Dr to Reservoir Rd	Bitumen Shared Path	875
							Mb12	Goldfields Reservoir Track	Upgrade Existing Walking Path to Gravel Shared Path	1,700	High, as a very highly used area	102,000	Goldfields Reservoir Track	Gravel Foot Path	1,700
							Mb29	Dundas Rd Hughes St to Fink St	On Road Path	300	Low	3,000			
													Dundas Rd Clarendon St to Hughes St	On Road Path	980
								Inkerman St From Gladstone St to Nolan St	On Road Path	1,750	2014		Inkerman St From Gladstone St to Nolan St	On Road Path	1,750
								Majorca Rd bw Nolan St & Gillies St	On Road Path	250	2014		Majorca Rd bw Nolan St & Gillies St	On Road Path	250

Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016		
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)
								Majorca Rd bw Nolan St & Ulbrick St	On Road Path in service lane	1,300	2012		Majorca Rd bw Nolan St & Ulbrick St	On Road Path Nolan St to Gillies St in service lane from Gillies St to Ulbrick St	1,300
													Sutton Road Basset St to Underpass	Concrete Shared Path	1,000
							Mb13	Napier St from Inkerman St to Lake Rd	On Road Path	675	High	4,000			
							Mb14	Tuaggra St Underpass to Napier St	On Road Path or Off Road as an alternative	330	High	2,000			
							Mb30	Kars St from Clarendon St to Goldsmith St	Footpath	570	Low	85,500			
							Mb31	Inkerman St from Napier St to Alma St	Footpath	70	High	10,500			
							Mb32	Kars St from Burns St to Burke St	Footpath	70	High	10,500			

Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016		
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)
							Mb33	Victoria St from Mariners Reef Rd to School Entrance	Footpath	110	Medium	16,500			
							Mb34	Mariners Reef Rd from Victoria St to Railway Line	Footpath	250	High	37,500			
							Mb35	Mariners Reef Rd and Lean St from Railway line to Derby Rd	Footpath	1,025	Low	153,750			
							Mb36	Goldsmith Street form Inkerman St to Kars St	Footpath	231	Low	34,650			
							Mb38	Napier St from Derby Rd to Clark St	Footpath	50	Low	7,500			
							Mb39	Nelson St from Majorca Rd to Gibbs St	Footpath	365	Medium	54,750			
							Mb40	Barr St from Gillies St to Bryant St	Footpath	380	Medium	57,000			
							Mb41	Fraser St from Gillies St to Sutton Rd	Footpath	260	Medium	39,000			

Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016		
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)
							Mb42	Gillies St from Taylor St to Spring St	Footpath	190	Low	28,500			
							Mb43	Spring St from Gillies St to Mb Community House	Footpath	370	Low	55,500			
							Mb44	Goldenwattle Dr from Tatlor St to Kennedy St	Footpath	600	Low	90,000			
							Mb45	Harkness St from Burns St to Havilah Entrance	Footpath	90	High	13,500			
							Mb46	Burns St from Tullaroop Rd to Sutton Road	Footpath	1,520	High	228,000			
							Mb47	Lake Rd from Swimming Pool to Earl St Napier St intersection	Footpath	170	High	34,000			
							Mb48	Carrick St from Hill View Ave to Gartley Crt	Footpath	50	Low	7,500			
							Mb49	Nelson St from Majorca Rd to Holden St	Footpath	95	Low	14,250			

Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016		
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)
							Mb50	Majorca Rd bw Gillies to Nelson	Relocate On Road Path in service lane onto Majorca Rd - both sides	625	High	15,000			
Carisbrook															
C1	Carisbrook to Maryborough Off Road Path	Extend Off Road Gravel Path to Smith St	1,000	High	2004	30,000		Carisbrook to Maryborough Off Road Path Potts Lane to Simson St	Extended Off Road Asphalt Path from Potts Lane to Victoria St	1,550	2012		Carisbrook to Maryborough Off Road Path Potts Lane to Simson St	Off Road Asphalt Path	1,550
								Carisbrook to Maryborough Off Road Path Victoria St to McCallum St	Extended Off Road Concrete and Asphalt Path from along Victoria St to McCallum St	735	2012		Carisbrook to Maryborough Off Road Path Victoria St to Bucknall St	Concrete Shared Path	550
													Carisbrook to Maryborough Off Road Path Bucknall St to McCallum St	Asphalt Shared Path	285



Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016		
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)
								Carisbrook to Maryborough Off Road Path Potts Lane to Basset St	Upgraded Asphalt Shared Path from Potts Lane to Basset St Maryborough	3,700	2012		Carisbrook to Maryborough Off Road Path Potts Lane to Basset St	Asphalt Shared Path	3,700
								Bland & Bucknall Reserve Green St to McCallum St	Upgraded Gravel Shared Paths And Pedestrian Bridge	200	2007		Bland & Bucknall Reserve Green St to McCallum St	Gravel Shared Path	200
C2	Hare St to Recreation Reserve & along Birch St to Pyrenees Hwy	On Road Path	1,000	High	2005	4,000		Not Required							
C3	Bucknall St from Pyrenees Hwy to Hare St & McNeill St bw Bucknall St & Birch St	On Road Path	1,330	Low	2011	5,200									

Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016		
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)
C4	Carisbrook Primary School to Brian Dowie Nature Reserve along Pyrenees Hwy	Off Road Gravel Path	1,500	High	2005	30,000	Cb8	Not Required as mostly completed under C1 Better to extend Simson St & Baringhup Rd from McCallum St to Mortlock St (west Side)	Off Road Shared Gravel	600	Low	30,000			
							Cb1	Carisbrook Primary School from Camp St School Crossing to Victoria St School Crossing	Concrete and Asphalt Shared Path	120	High	24,000			
							Cb2	Simson St & Landrigan Rd from Camp st to Leisure Centre	Off Road Shared Path and Pedestrian Bridge	200	Low	60,000			

Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016		
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)
							Cb3	Urquart St from Pyrenees Hwy to Recreation Reserve McNeill St (missing sections)	Off Road Shared Path	520	Low	104,000			
							Cb4	Birch St from Green St to McNeil St	Footpath	375	Low	56,250			
							Cb5	Green St from Bucknall st to Tullaroop Creek	Footpath	130	High	19,500			
							Cb6	Hunter St from Simson St to Grice St	Footpath	320	Low	48,000			
							Cb7	McNeill St from Birch to Bucknall St	Footpath	450	Low	67,500			
							Cb9	Mc Callum St from Simson St to Hall St St	Footpath	125	Low	18,750			
							Cb10	Bucknall St from McLaughlan St to McNeil St	Footpath	120	High	18,000			
							Cb11	Smith & McNeill St from Victoria St to Birch St	Footpath	560	Low	84,000			

Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016		
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)
Dunolly															
D1	Bridgewater Dunolly Rd along Elgin St from Hospital St to Broadway (north side)	Extend Off Road Gravel Path	200	High	2004	3,500		Bridgewater Dunolly Rd along Elgin St from Hospital St to Broadway (north side)	Extended Off Road Asphalt Path	660	2012		Bridgewater Dunolly Rd along Elgin St from Hospital St to Broadway (north side)	Asphalt Shared Path	660
D2	Maryborough to Dunolly via Timor	Off Road Gravel Path	18.5 km Off Road & 1,500 m On Road	Medium	2007	320,000		Not Required as semi On Road Cycling Routes Established							
D3	Bridgewater Dunolly Rd along Elgin St from Hospital St to Broadway (south side)	Extend Off Road Gravel Path	580	High	2006	10,100		Not Required as asphalt path on west side							
								Broadway from Thompson St to Bull St both sides	Upgraded existing footpaths from concrete to Heritage asphalt	240	2015		Broadway from Thompson St to Bull St both sides	Upgraded existing footpaths from concrete to Heritage asphalt	240
								Thompson St from Broadway to Barkly St	Asphalt Footpath	60	2011		Thompson St from Broadway to Barkly St	Asphalt Footpath	60

Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016		
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)
							Dn1	Hospital St from Elgin Rd to Thompson St	Off Road Shared Concrete	680	Medium	136,000			
							Dn12	Havelock Street Elgin Rd to Thompson St	On Road Path	525	Low	5,000			
							Dn2	Thompson St from Havelock St to Maude St	Off Road Shared Concrete	640	Medium	128,000			
							Dn3	Maude St Bull St to Thompson St	Footpath	210	Low	31,500			
							Dn4	Bull St Inkerman St to Maude St	Footpath	110	Medium	16,500			
							Dn5	Broadway from Bull St to Tweeddale St	Footpath Upgrade	150	High	22,500			
							Dn6	Tweeddale St from Broadway to Inkerman	Off Road Shared Concrete	60	Medium	12,000			
							Dn7	Inkerman St from Tweeddale St to Thompson St	Off Road Shared Concrete	400	Medium	80,000			

Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016		
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)
							Dn8	Bull St from Broadway to Havelock St	Off Road Shared Concrete	350	Low	70,000			
							Dn9	Market St from Bull St to Swimming Pool	Off Road Shared Concrete	100	Low	20,000			
							Dn10	Pug Hole Delideo Reserve	Footpath Upgrade	540	Medium	32,400			
							Dn11	Havelock Street Elgin Rd to Thompson St	Footpath	525	Low	78,750			
Talbot															
T1	Maryborough to Ballarat St North in Talbot	Off Road Gravel Path	11km Off Road & 300m On Road	Medium	2009	200,000		Not Required as semi On Road Cycling Routes Established							

Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016		
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)
T2	Ballarat St North bw Crespigny St & Scandinavian Cr & along Scandinavian Cr & Bond St to Ballarat Maryborough Rd	On Road Path	1,750	High	2005	7,000	Ta1	Ballarat St North / Heales St bw Crespigny St & Camp St, then Camp bw Heales St & Scandinavian Cr & along Scandinavian Cr & Bond St to Ballarat Maryborough Rd	On Road Path	1,750	High	15,000			
T3	Camp St bw Scandinavian Cr to Barnes St	On Road Path	1,000	High	2005	4,000	Ta2	Camp St bw Scandinavian Cr to Barnes St	Needs to be Off Road Path with Bridge crossing	550	Medium	50,000			
								Scandinavian Cr from Ballarat St to Fraser St west side & to Bell St east side.	Upgraded existing footpaths to concrete	360	2014		Scandinavian Cr from Ballarat St to Fraser St west side & to Bell St east side.	Upgraded existing footpaths to concrete	360
								Camp St bw Scandinavian Cr & Heales St	Upgraded existing footpaths to concrete both sides	60	2014		Camp St bw Scandinavian Cr & Heales St	Upgraded existing footpaths to concrete both sides	60

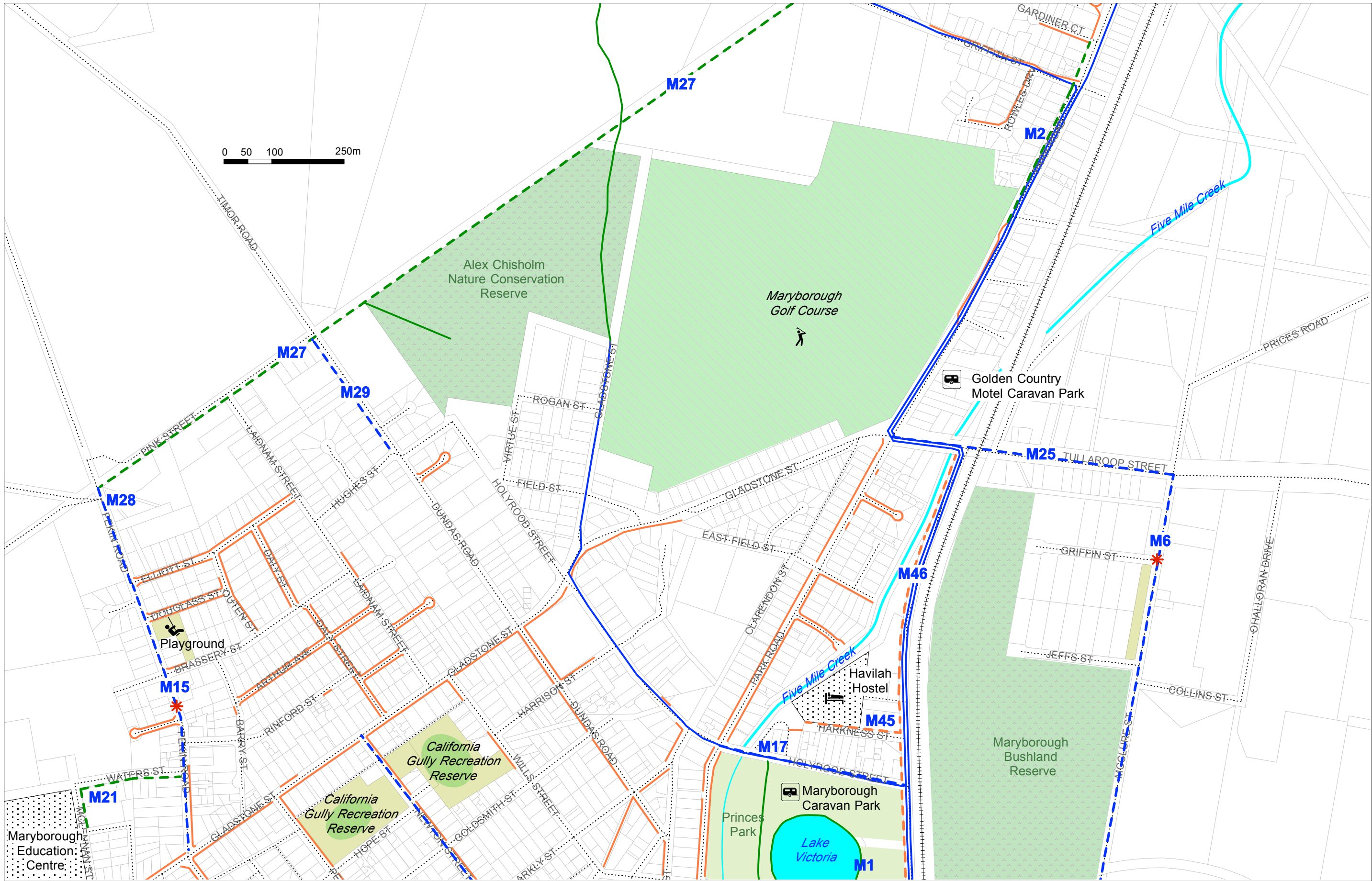
Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016		
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)
								Ballarat St bw Scandinavian Cr & Heales St	Upgraded existing footpath to concrete south side	60	2014		Ballarat St bw Scandinavian Cr & Heales St	Upgraded existing footpath to concrete south side	60
							Ta3	Ballarat St from Star St to Railway St	Footpath	50	High	7,500			
							Ta4	Prince Alfred St from Ballarat North St to Rowe St	Footpath	160	High	24,000			
							Ta5	Ballarat North St from Ohara St to King St	Footpath	400	Low	60,000			
							Ta6	King St from Ballarat North St to Ballarat Mb Rd	Footpath	250	Low	37,500			
Bealiba															
B1								Grant St from Main Rd to Bealiba Primary School	Asphalt and Concrete Footpath	120	2013		Grant St from Main Rd to Bealiba Primary School	Asphalt Footpath	120



Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016			
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)	
							Be1	Cochrane St & Mount Rd from Davies St to Scent Farm Road (Bealiba Reservoir)	Off Road Gravel Path	3,000	Low	180,000				
							Be2	Cemetery Rd from Mount Rd to Cemetery	Off Road Gravel Path	510	Low	30,600				
							Be3	Main St from Grant St to Davies St	Footpath	100	High	15,000				
							Be4	Davies St from Cochrane St to Grant St	Footpath	150	High	22,500				
							Be5	Davies St from Grant St to Main St	Footpath	200	Low	30,000				
							Be6	Bicycle Parking Bealiba Hotel Main St	Bicycle Parking		Medium	2,000				
TOTALS						\$ 853,250				60,681		\$ 3,935,150			27,465	
										<b>Length (m)</b>		<b>Estimated Cost \$</b>				
										<b>Completed Works to April 2016</b>	19,190					
										<b>Required Cycle/Pedestrian PathWorks</b>	30,100	Estimated Cost	2,264,600			


Proposed Bicycle Path Works 2003-2013								Completed Works to April 2016 / Required Cycle/Pedestrian Works/ Required Pedestrian Paths					Existing Bicycle Paths April 2016		
2003/13 Strategy Code	Location	Works	Length (m)	Priority	Suggested Year	Estimated Cost (\$)	Required Works Code	Location	Works	Length (m)	Year/ Priority	Estimated Cost (\$)	Location	Type	Length (m)
								Required Pedestrian Paths		11,391	Esimat- ed Cost	1,670,550			

## 14.0 Maps



**Maryborough Township - Paths & Trails Map A**

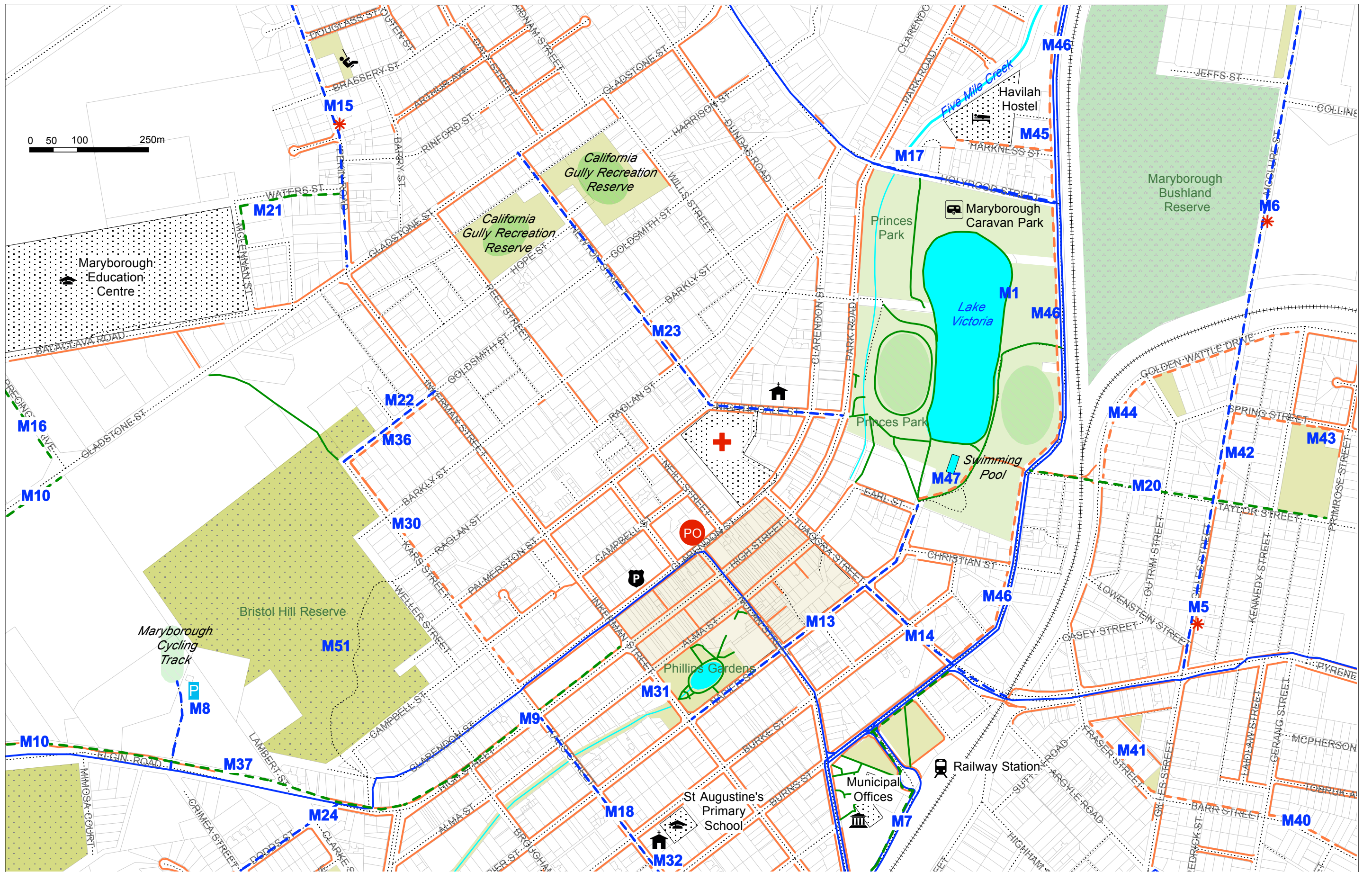
Scale  
1:7500 @ A3



**Legend:**

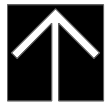
- |                        |                        |                       |              |               |           |
|------------------------|------------------------|-----------------------|--------------|---------------|-----------|
| Existing Off-Road Path | Proposed Off-Road Path | Recreation Reserve    | Waterway     | Works Code    | <b>M3</b> |
| Existing Footpath      | Proposed Footpath      | Conservation Parkland | Town Feature | Signage /Line | *         |
| Existing On-Road       | Proposed On-Road       | Bushland Reserve      | Rail Line    | Marking       | ++++      |
| Bicycle Lane           | Bicycle Lane           | Recreation Facility   | Roadway      | Town Centre   | .....     |





Maryborough Township - Paths & Trails Map B

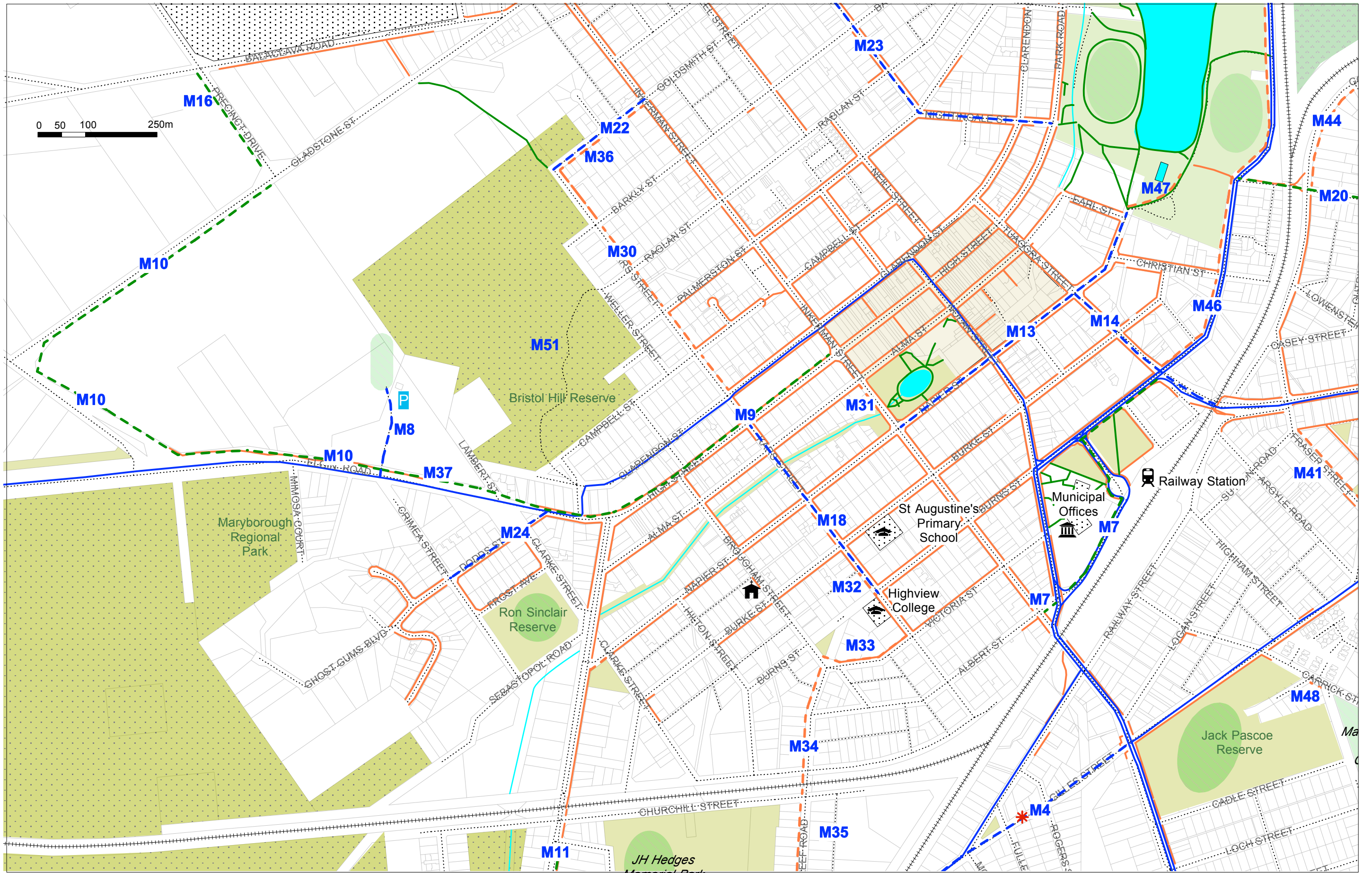
Scale  
1:7500 @ A3



**Legend:**

Existing Off-Road Path	Proposed Off-Road Path	Recreation Reserve	Waterway	Works Code	<b>M3</b>
Existing Footpath	Proposed Footpath	Conservation Parkland	Town Feature	Signage /Line	*
Existing On-Road	Proposed On-Road	Bushland Reserve	Rail Line	Marking	++++
Bicycle Lane	Bicycle Lane	Recreation Facility	Roadway	Town Centre	.....





Maryborough Township - Paths & Trails Map C

Scale  
1:7500 @ A3



**Legend:**

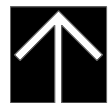
Existing Off-Road Path	Proposed Off-Road Path	Recreation Reserve	Waterway	Works Code	M3
Existing Footpath	Proposed Footpath	Conservation Parkland	Town Feature	Signage /Line	*
Existing On-Road	Proposed On-Road	Bushland Reserve	Rail Line	Marking	++++
Bicycle Lane	Bicycle Lane	Recreation Facility	Roadway	Town Centre	.....





**Maryborough Township - Paths & Trails Map D**

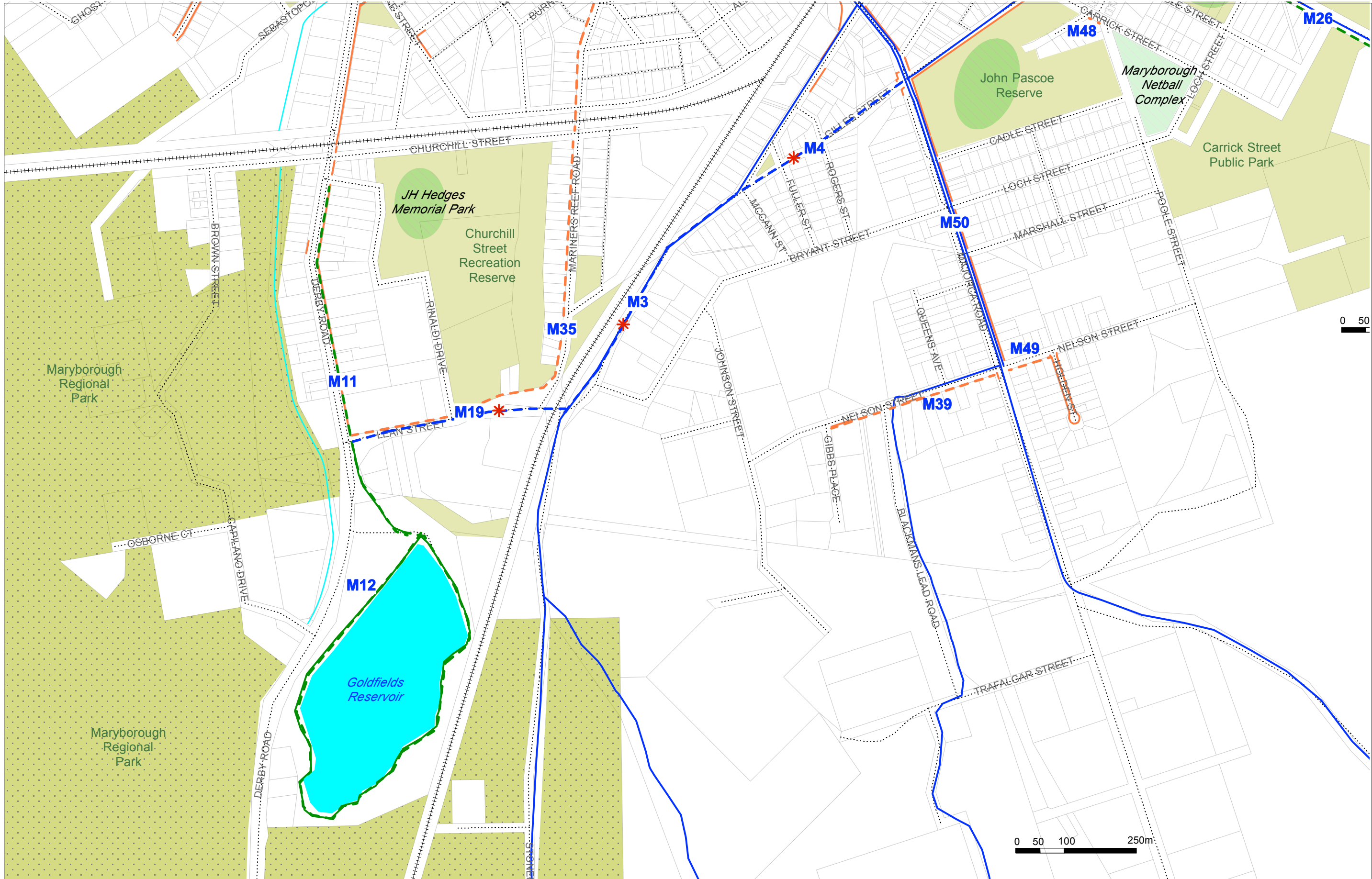
Scale  
1:7500 @ A3



**Legend:**

Existing Off-Road Path	Proposed Off-Road Path	Recreation Reserve	Waterway	Works Code	<b>M3</b>
Existing Footpath	Proposed Footpath	Conservation Parkland	Town Feature	Signage /Line	*
Existing On-Road	Proposed On-Road	Bushland Reserve	Rail Line	Marking	++++
Bicycle Lane	Bicycle Lane	Recreation Facility	Roadway	Town Centre	.....





Maryborough Township - Paths & Trails Map E

Scale  
1:7500 @ A3



**Legend:**

Existing Off-Road Path	Proposed Off-Road Path	Recreation Reserve	Waterway	Works Code	<b>M3</b>
Existing Footpath	Proposed Footpath	Conservation Parkland	Town Feature	Signage /Line	*
Existing On-Road	Proposed On-Road	Bushland Reserve	Rail Line	Marking	++++
Bicycle Lane	Bicycle Lane	Recreation Facility	Roadway	Town Centre	.....



**Carisbrook Township - Paths & Trails Map**

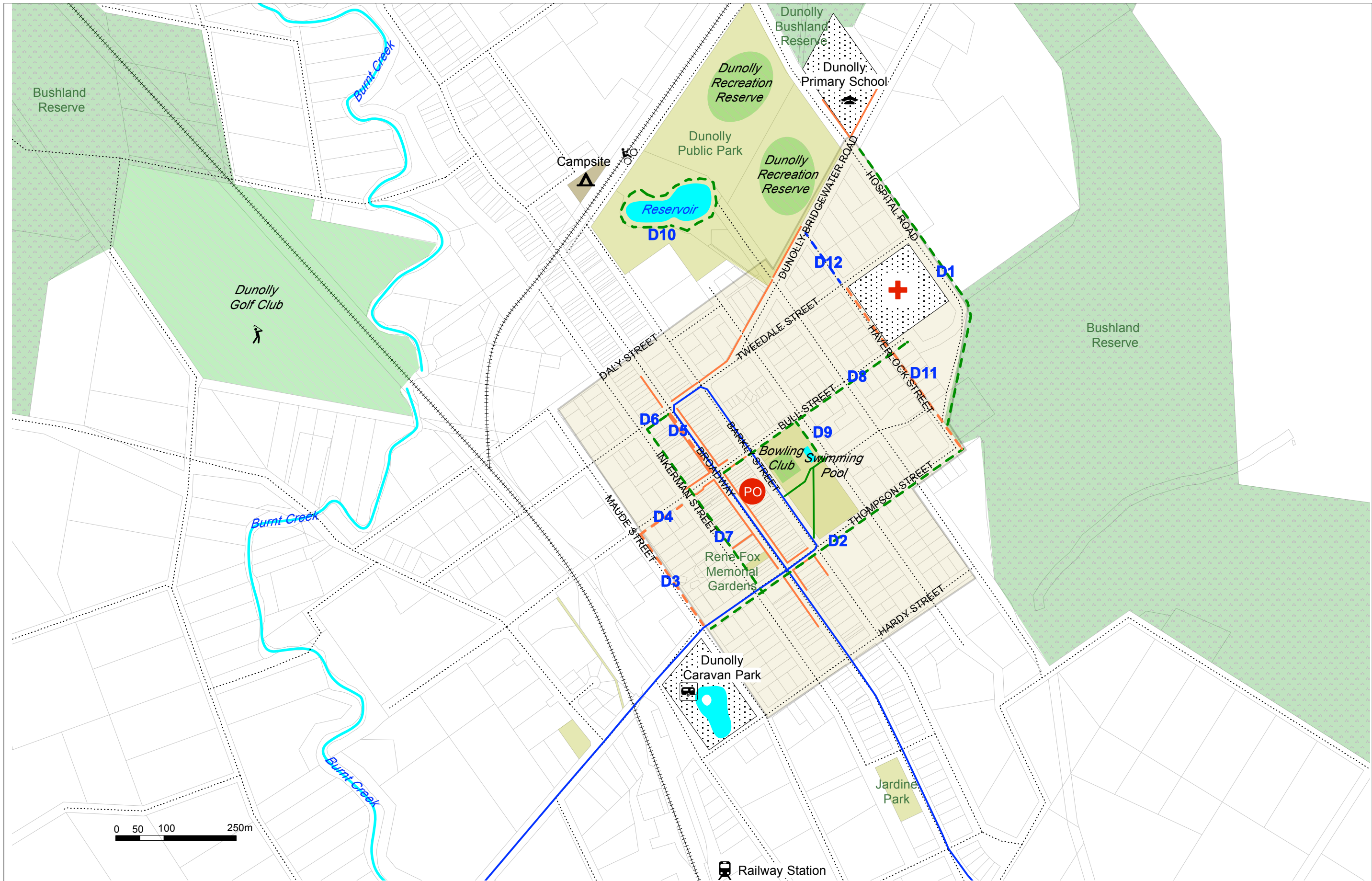
Scale  
1:7500 @ A3



**Legend:**

Existing Off-Road Path	Proposed Off-Road Path	Recreation Reserve	Waterway	Works Code	<b>M3</b>
Existing Footpath	Proposed Footpath	Conservation Parkland	Town Feature	Signage /Line	*
Existing On-Road	Proposed On-Road	Bushland Reserve	Rail Line	Marking	++++
Bicycle Lane	Bicycle Lane	Recreation Facility	Roadway	Town Centre	.....





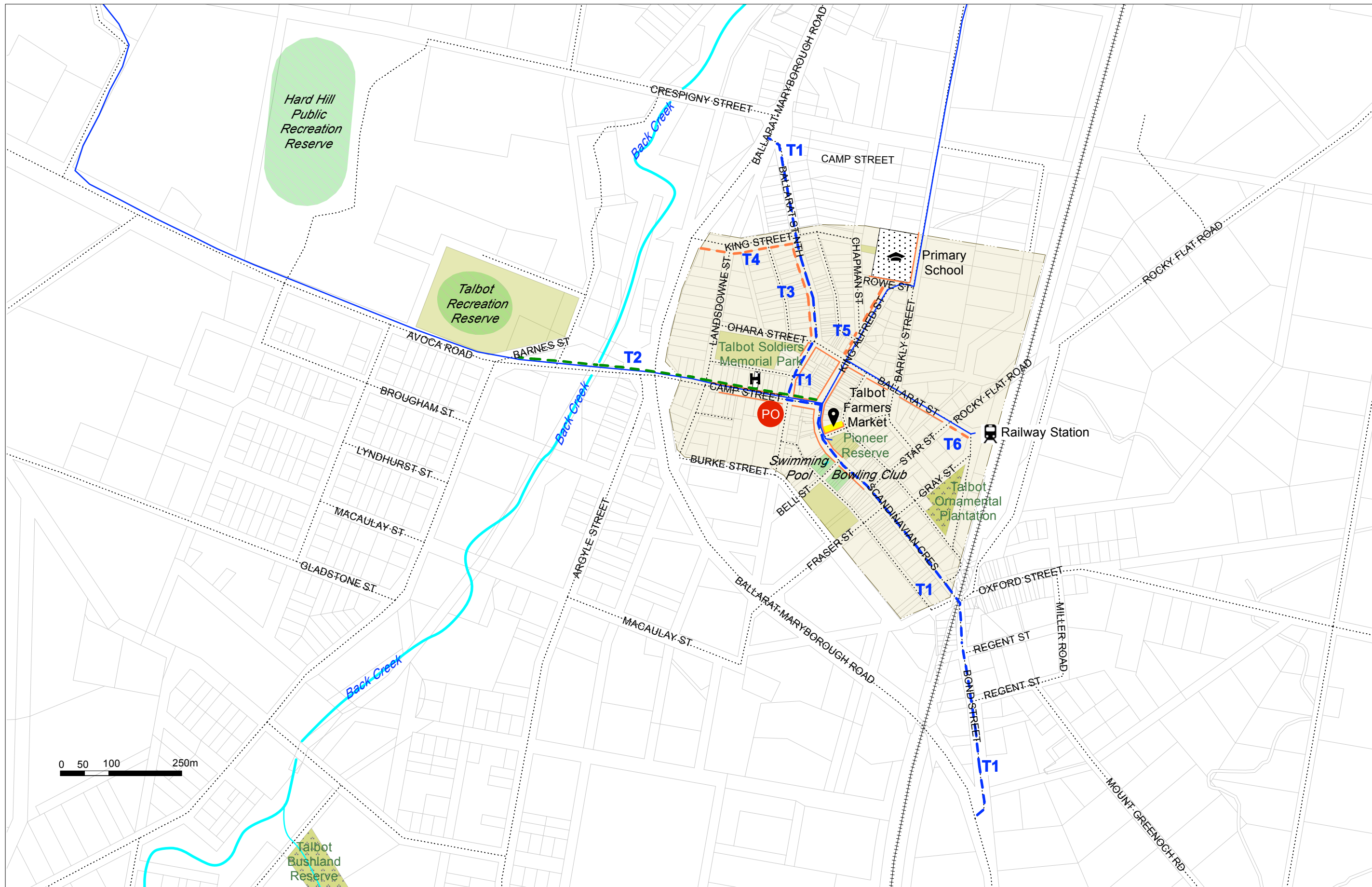
**Dunolly Township - Paths & Trails Map**

Scale  
1:7500 @ A3



**Legend:**

Existing Off-Road Path	Proposed Off-Road Path	Recreation Reserve	Waterway	Works Code	<b>M3</b>
Existing Footpath	Proposed Footpath	Conservation Parkland	Town Feature	Signage /Line	*
Existing On-Road	Proposed On-Road	Bushland Reserve	Health Service	Marking	+
Bicycle Lane	Bicycle Lane	Recreation Facility	Roadway	Town Centre	.....



**Talbot Township - Paths & Trails Map**

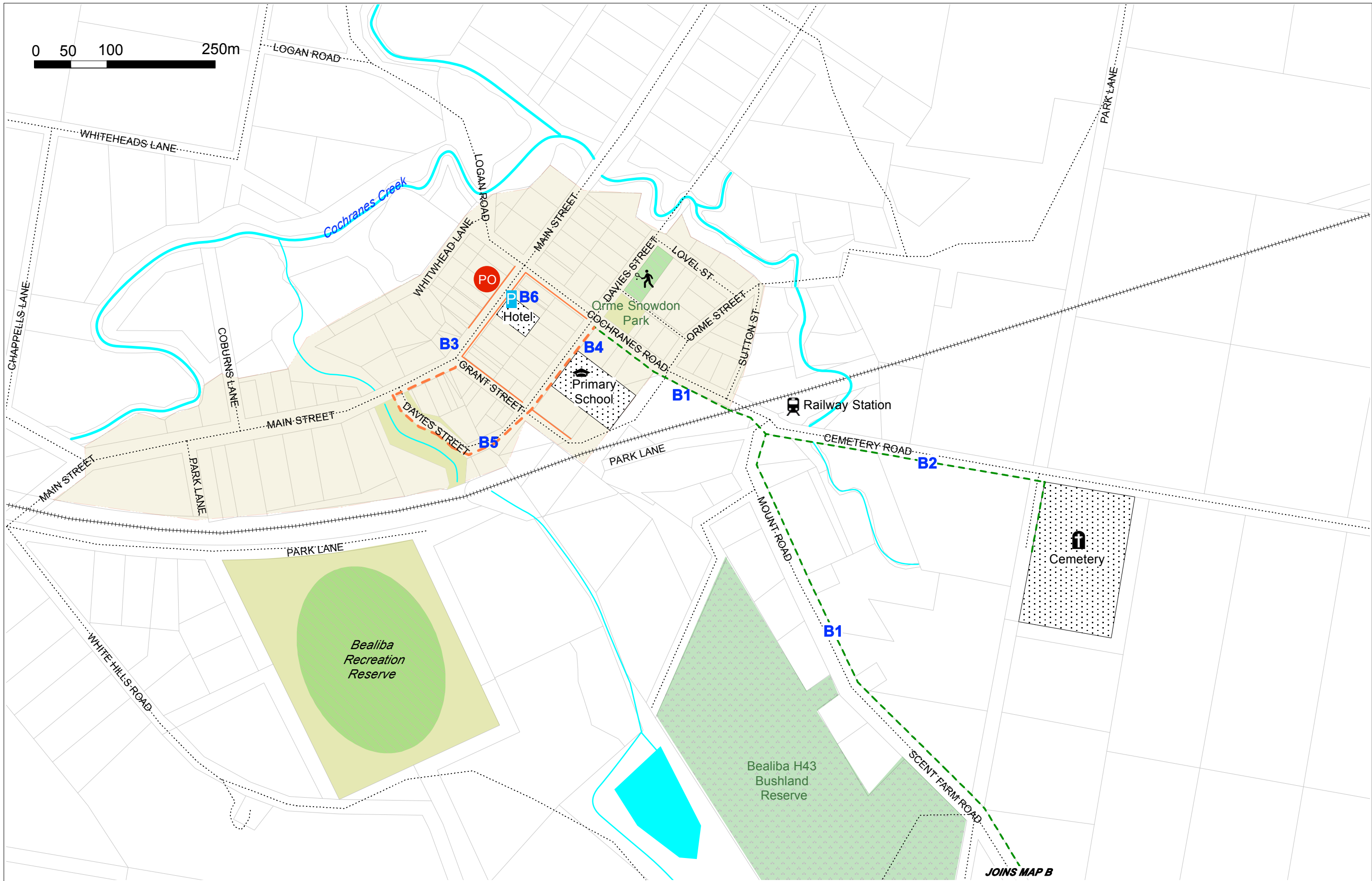
Scale  
1:7500 @ A3



**Legend:**


Existing Off-Road Path	Proposed Off-Road Path	Recreation Reserve	Waterway	Works Code	<b>M3</b>
Existing Footpath	Proposed Footpath	Bushland Reserve	Town Feature	Signage /Line	*
Existing On-Road	Proposed On-Road	Bushland Reserve H3	Railway Line	Marking	+++
Bicycle Lane	Bicycle Lane	Recreation Facility	Roadway	Town Centre	.....





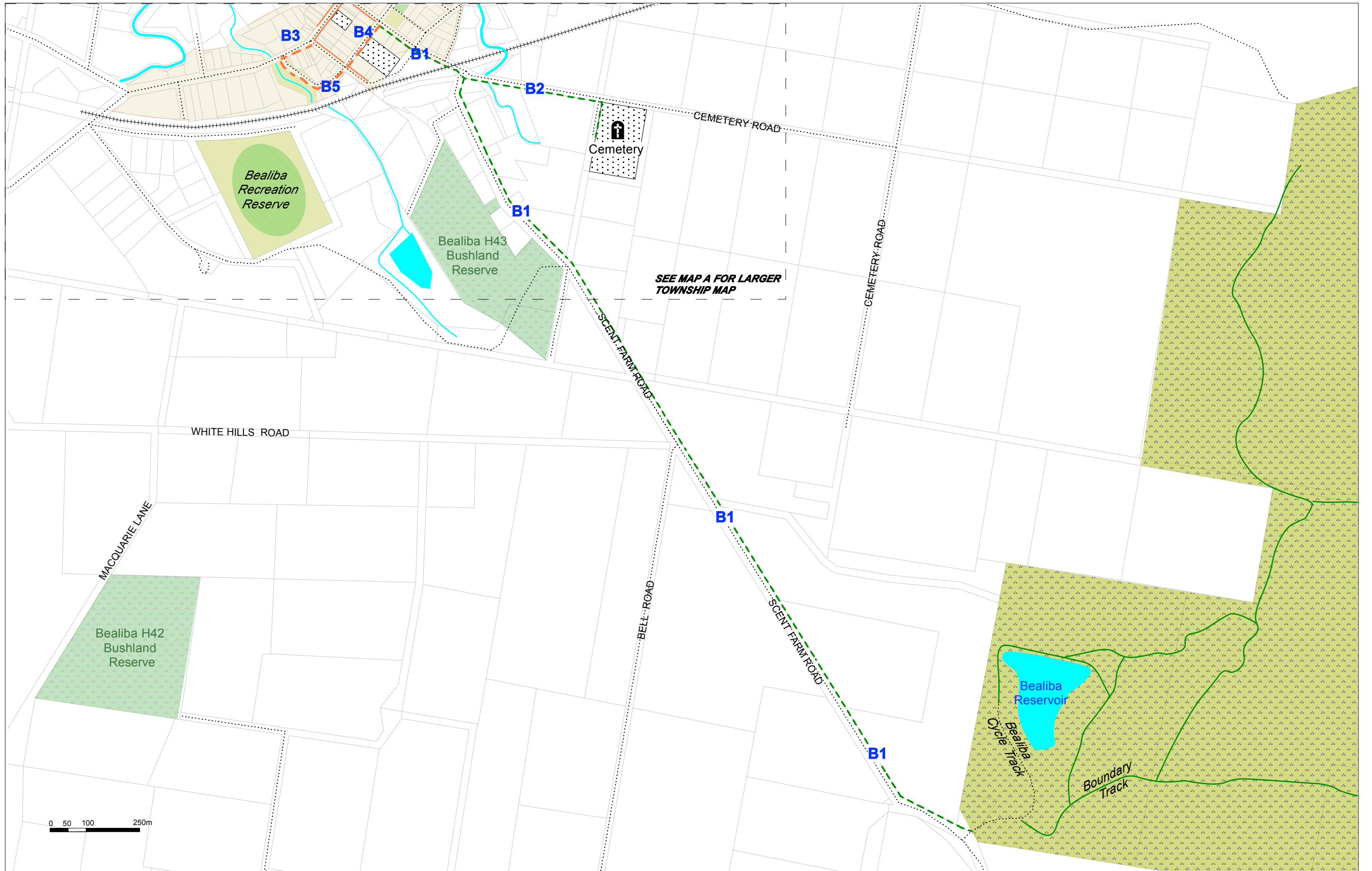
**Bealiba Township - Paths & Trails Map A**

Scale  
1:5000 @ A3




**Legend:**

Existing Off-Road Path	Proposed Off-Road Path	Recreation Reserve	Waterway	Works Code	<b>M3</b>
Existing Footpath	Proposed Footpath	Conservation Parkland	Town Feature	Signage /Line	*
Existing On-Road	Proposed On-Road	Bushland Reserve	Town Centre	Marking	
Bicycle Lane	Bicycle Lane	Recreation Facility	Roadway	Bike Parking	P



**Bealiba Township - Paths & Trails Map B**

Scale  
1:10000 @ A3



**Legend:**

Existing Off-Road Path	Proposed Off-Road Path	Recreation Reserve	Waterway	Works Code	<b>M3</b>
Existing Footpath	Proposed Footpath	Conservation Parkland	Town Feature	Signage /Line	*
Existing On-Road	Proposed On-Road	Bushland Reserve	Town Centre	Marking	
Bicycle Lane	Bicycle Lane	Recreation Facility	Roadway	Bike Parking	P